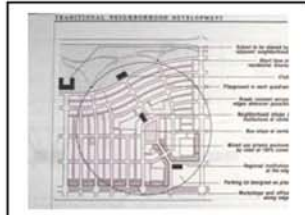


### Key Design Elements: Traditional Towns and Traditional Neighborhoods

Unlike conventional suburban development patterns (with separated land uses, deep setbacks, no on-street parking, cul-de-sacs, and no sidewalks), Traditional Towns and Traditional Neighborhoods promote more compact, walkable, mixed-use, interconnected and sustainable communities, and have the following Key Design Elements:



**“Anchor” for the Neighborhood:** features a Green, Park, Corner Store, Post Office, Library, Town Hall, Community Center, Train Station, Theatre, or other Civic Use; enjoys success along a 3 to 5 block long “main street” or in a neighborhood or town center; provides a place for special events



**Service Area and Size:** features a ¼ to ½ mile (5 to 10 minute walk) from the Neighborhood Center to the edge; creates a range of 40 to 160 acres for each neighborhood



**Mix of Uses:** combines Residential, Commercial, Institutional, Limited Industrial, Recreational and Open Space uses in a diversified but seamless arrangement; also combines first floor retail with second floor apartments and/or offices in the town/neighborhood center; encourages live-work units and granny flats as Accessory Dwelling Units



**Park, Open Space, Countryside:** creates the green, square or park to enhance and beautify the Town/Neighborhood Center and neighborhood; a system of “green spaces” ecologically balanced with the built environment and distributed within the community; includes a “green edge” of open space to help shape neighborhoods and towns; forms the countryside between towns, villages, and other places



**Network System of Interconnected Streets:** organizes a block and pattern of lots; integrates boulevards, avenues, neighborhood streets, and alleys; links to pedestrian and other motorized and non-motorized transportation systems; streets and street walls help to create outdoor rooms and the streetscape; street vistas terminate with public space, landmark structures or civic buildings



**On-Street/Parallel Parking:** provides a separator between vehicular and pedestrian traffic; utilizes cartway as an “aisle”; (with “overflow” parking to the rear or side of buildings); promotes effective “traffic calming” by slowing down the speed of vehicles, especially along narrower streets



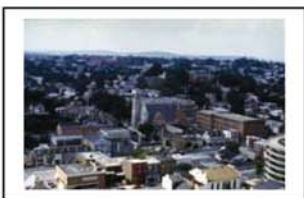
**Streetscape:** promotes human scale relationship for the pedestrian as part of the public realm; an “outdoor room” type of space created by 2 to 5 story buildings, located (in the most compact part of the Transect) in the range of 60 to 85 feet across from one another on both sides of the street; buildings at a “build-to” line create a Street Wall (which may have up to a 4 to 8 foot offset)



**Lanes (Alleys):** allows for preservation of frontage streetscape; provides vehicular access to parking in the rear; provides opportunities for rear access to an accessory apartment (granny flats), or for deliveries; provides access for utilities and staging construction



**Sidewalks/Crosswalks/Pedestrian Paths/Walkways:** serve to link uses, buildings, lots and streets together; accommodates a healthy pedestrian circulation network; provides close to home opportunities for exercise; enhances wayfinding and an appreciation of the neighborhood/place



**Building Types:** focuses on buildings designed by type, not solely by function, to allow for adaptations and changes in use (e.g. from dwelling, to shop, to work place, to civic use); most appropriate when an expression of regional/local context and style



**Porch/Portico/Colonnade:** serves as transition element from the private realm of the building to public realm of the sidewalk and street; provides shade; promotes a finer, more ornamental “texture” of the building; creates a cozy space to sit, walk, relax; provides the outdoor room for greeting and socializing with neighbors and friends



**Shade Trees:** provide (as street trees) the canopy/overhead plane to help create an “outdoor room”; and (as shade trees) provides an “old shade” character of the neighborhood



**Other Vertical Infrastructure:** includes Civic Art such as gateways, monuments, gazebos, pavilions, pergolas, as well as walls, fences, trees, hedges, street lamps, benches, or like features