

## ZONING ORDINANCE

### *19 Attachment 4*

## **Chapter G-28**

### **Parking and Circulation Design Guidelines**

#### **Topics:**

- G-28.200 General Design Considerations.
- G-28.210 Access and Entries.
- G-28.220 Lighting.
- G-28.230 Hardscape.
- G-28.240 Circulation.
- G-28.250 Landscaping, Screening, and Buffering.
- G-28.260 Parking Structures.

#### **G-28.200 General Design Considerations.**

The primary design considerations for planning parking lot layouts and site access are safety and efficiency, minimizing the visual impact of large paved areas, reinforcing the street environment, maximizing pedestrian orientation, integrating parking into overall site development, and humanizing by means of thoughtful lighting, landscape, hardscape, and parking layout.

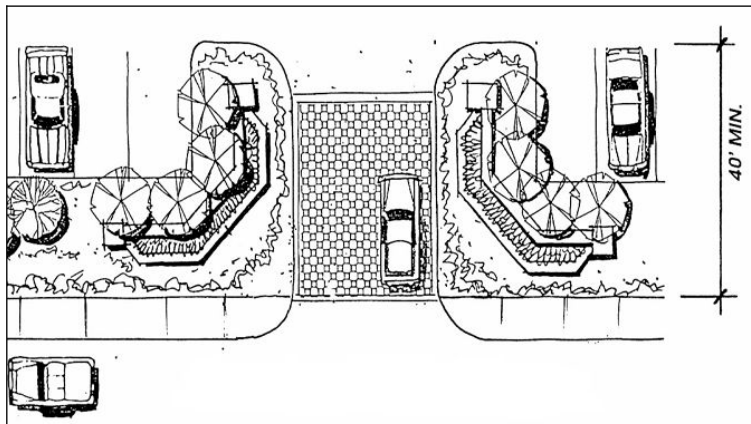
- A. The following factors should be considered in the design and development of off-street parking areas:
  - 1. Ingress and egress with consideration to possible conflicts with vehicular and pedestrian traffic;
  - 2. Intra-parking lot pedestrian and vehicular conflicts;
  - 3. Reinforcing the street edge and a pedestrian environment;
  - 4. On-site circulation and service vehicle zones;
  - 5. Overall configuration and appearance of the parking area;
  - 6. Promoting compatibility with adjacent land uses;
  - 7. Minimizing opportunities for crime and undesirable activities through natural surveillance, access control and activity support;
  - 8. Potential to connect parking lot with neighboring properties;
  - 9. Shading the parking lot by means of canopy trees and other landscaping;
  - 10. Creating a sense of spatial organization and experiential meaning through the layout of the design of the parking lot;
  - 11. Reducing heat gain by shading pavement and employing light-colored pavement and walls; and
  - 12. Maximizing on-site percolation and minimizing runoff to storm drains.
- B. Parking areas should be separated from buildings by either a raised concrete walkway or landscaped strip. If space allows, situations where parking stalls directly abut buildings should be avoided.

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- C. Lighting, landscaping, hardscape, fencing, parking layout and pedestrian paths should all contribute to the strength and clarity of the parking lot as a deliberate and thorough design.

### G-28.210 Access and Entries.

- A. Pedestrian and vehicular entrances must be clearly identified and easily accessible to minimize pedestrian/vehicle conflict and to create a sense of arrival. The use of enhanced paving, landscaping, and special architectural features and details is strongly encouraged to distinguish entrances.
- B. Parking lots adjacent to a public street should include a point of entry and clear and safe access for pedestrians from the sidewalk to the entrance of the building(s).



**FIGURE PD-1**  
**Parking Lot Entry**

### G-28.220 Lighting.

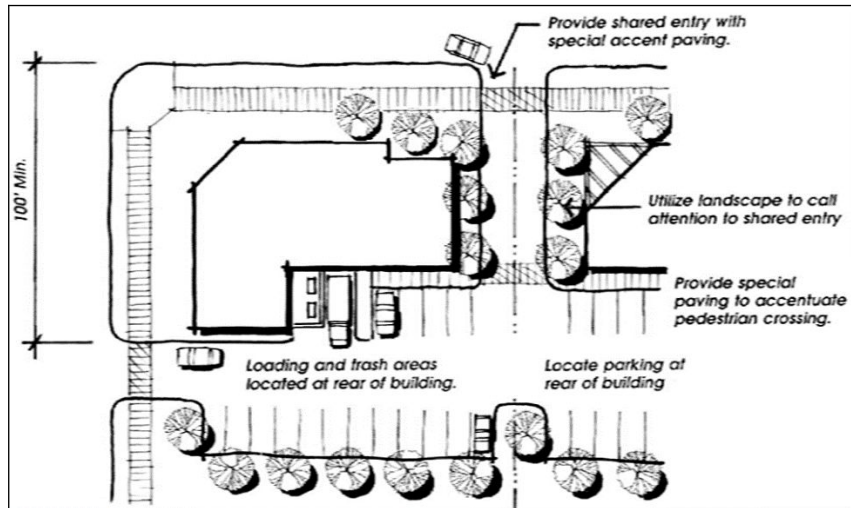
Carefully designed lighting is an essential element of good parking lot design. The placement, type, height, number and bulb type of lights should all contribute to a coherent lighting design. (See "Lighting" under Commercial and Public Use Design Guidelines.)

### G-28.230 Hardscape.

- A. A well thought-out selection and composition of hardscape materials can help to order space and reinforce the relationship of the parking lot to its surroundings and to the buildings it serves. Entrance and exit areas, areas that are the central focus of the parking lot design, major axis and areas that act as forecourts for entrances may be suitable locations for special paving materials such as brick or stamped asphalt. Alternative surfaces to asphalt and concrete paving for parking and loading areas such as grasscrete or stabilized decomposed granite are encouraged. The materials used should aim to lessen heat gain,

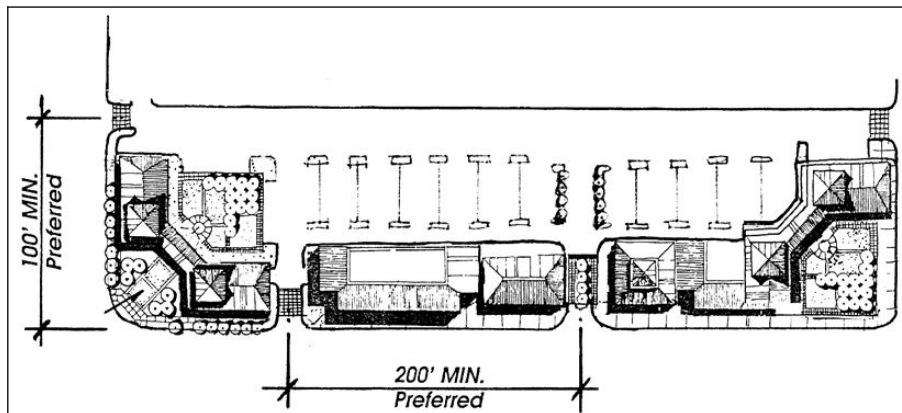
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increase on-site drainage and permeability, meet aesthetic qualities and respond to any applicable historic guidelines for the property and its setting.



**FIGURE PD-2**  
**Hardscape Materials**

- B. Limit curb cuts for driveways opening to public streets. Common shared access driveways which provide access to more than one site are encouraged to reduce the number of driveway entries along commercial streets.

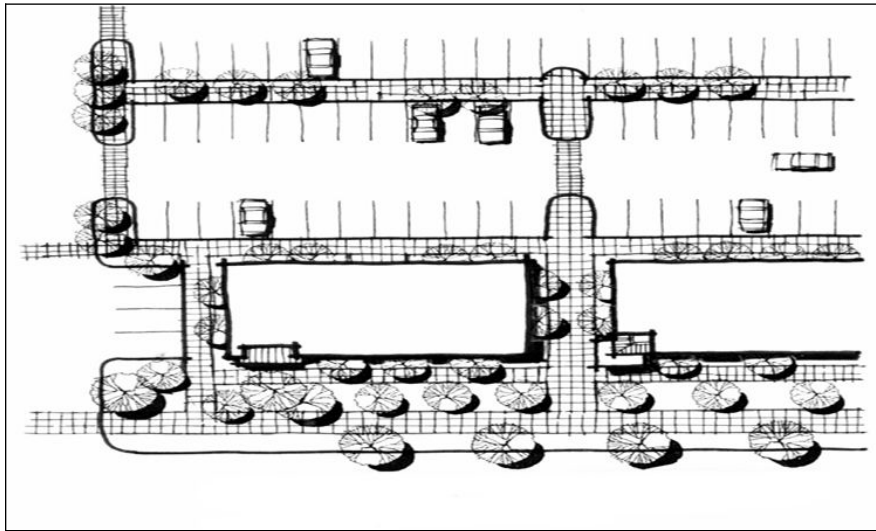


**FIGURE PD-3**  
**Spacing of Driveway Curb Cuts\***

- C. Design parking areas so that pedestrians walk parallel to moving cars. Minimize the need for the pedestrian to cross parking aisles and landscape areas.
- D. Access for service vehicles, trash collection and storage areas should be located on alleys where alleys exist.

**G-28.240 Circulation.**

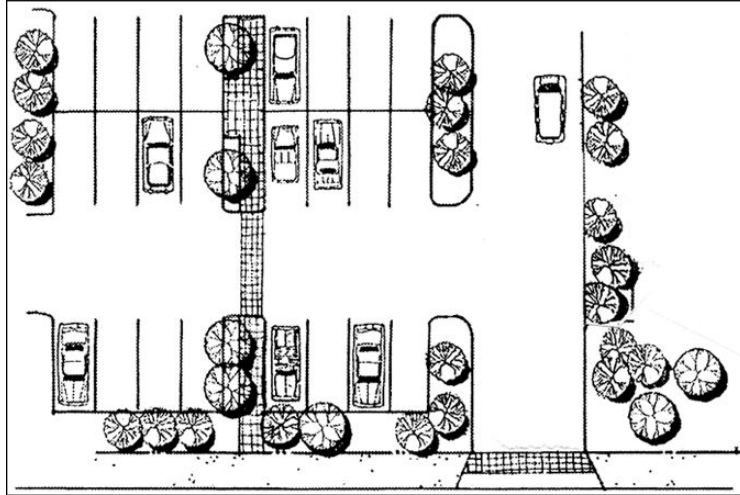
- A. Separate vehicular and pedestrian circulation systems should be provided where possible. The layout of parking areas should be designed so that pedestrians walk parallel to moving cars, minimizing the need to walk between parked cars or to cross parking aisles and landscape areas.
- B. Pedestrian linkages between uses in commercial developments should be emphasized. Parking lot designs should include walkways and planting that help direct pedestrians comfortably and safely to their destinations.
- C. Parking aisles should be separated from primary vehicle circulation routes whenever possible.



Provide significant pedestrian design elements with connections to adjacent buildings, parking and transit facilities

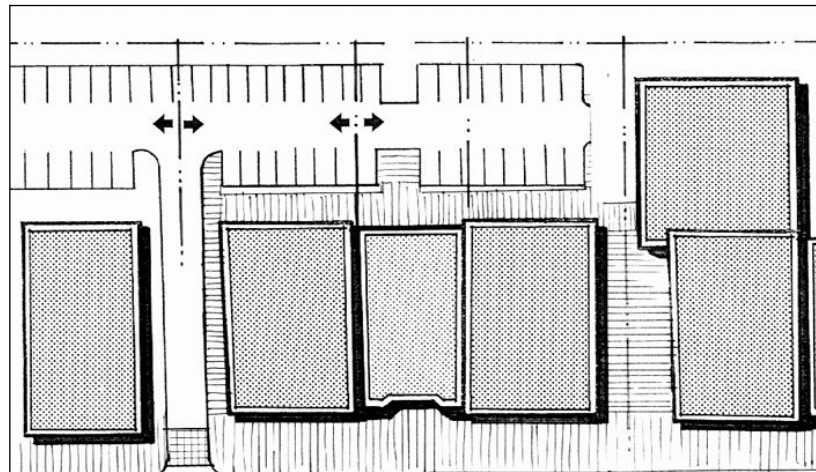
**FIGURE PD-4  
Pedestrian Design Elements**

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**FIGURE PD-5**  
**Separate Primary Vehicle Circulation From Parking Aisles**

- D. Where parking areas are connected, interior circulation should allow for a similar direction of travel and parking spaces in all areas to reduce conflict at points of connection.



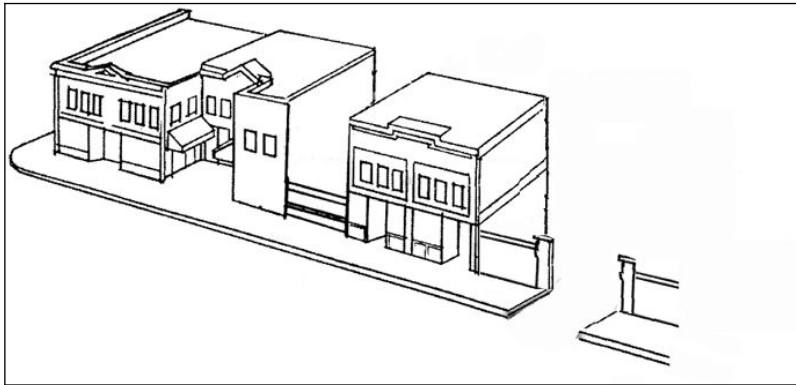
**FIGURE PD-6**  
**Shared Parking Access Should Minimize Circulation Conflicts**

**G-28.250 Landscaping, Screening, and Buffering.**

- A. Parking facilities should be landscaped with the following objectives in mind:
1. Visually break up large paved areas with landscaping;
  2. Maximize distribution of landscaping;
  3. Promote compatibility and function as a "good neighbor";

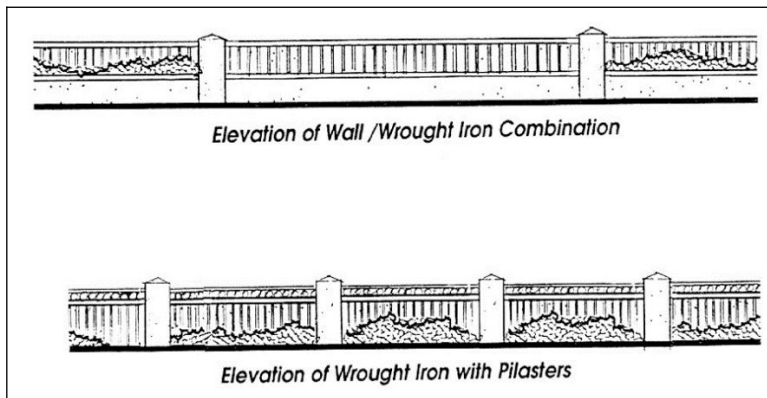
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- 4. Consider the use of trees planted at regular distances as a grove; and,
- 5. Shade 50 percent of the asphalt area within five years from time of installation.
- B. Parking lots adjacent to a public street should be landscaped to soften the visual impact of parked vehicles from the public right-of-way. Screening should consist of a combination of low walls and landscape materials. The objective is to strike a balance between screening parked automobiles and facilitate safety through natural surveillance opportunities.
- C. Walls viewed from the street or parking lot should be designed to complement the surrounding building architecture. (See "Fences and Hedges" in the Commercial and Public Use Design Guidelines.)



**FIGURE PD-7**  
**Design Walls to Integrate With Surrounding Architecture**

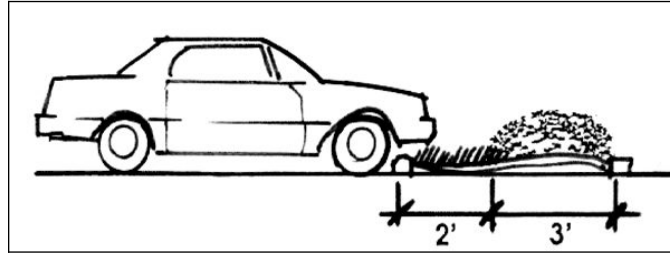
- D. As long as an open surveillance zone is maintained between 32 and 6 feet above grade, landscaping should be used in combination with walls and fences. The Zoning Ordinance requires a five-foot landscaped strip backed by a wall wherever parkways face on a public right-of-way or street.



**FIGURE PD-8**  
**Preferred Walls Design**

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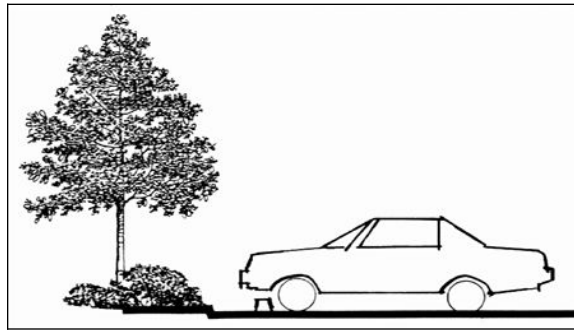
- E. Planting areas adjacent to parking areas, drives or walks should be protected by curbing where necessary, while allowing for drainage of surface water into permeable surface areas.



**FIGURE PD-9**

### **Protect Landscaping Through Curbing and Distance**

- F. Both perimeter and interior landscaping should incorporate canopy-type trees. The location and spacing of trees is dependent on the type of tree used, but the overall effect should be of a relatively consistent tree cover which will shade the pavement and vehicles. One tree for each 5 parking spaces is the recommended minimum.



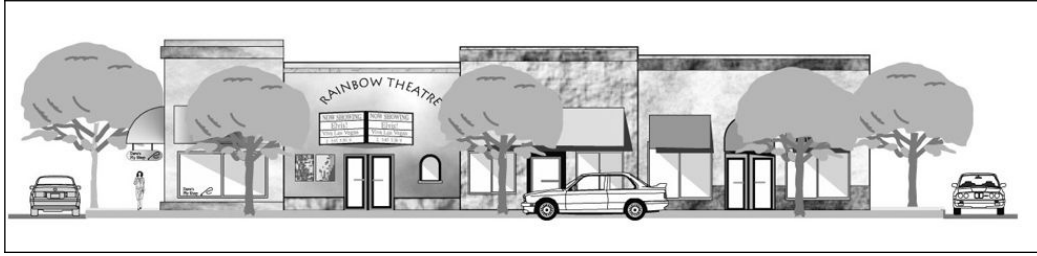
**FIGURE PD-10**

### **Tree Cover**

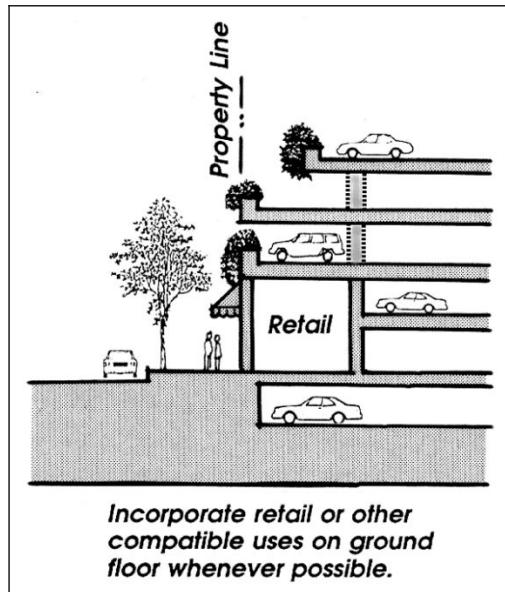
## **G-28.260 Parking Structures.**

- A. Due to the limited supply of land in West Hollywood, parking structures are encouraged which promote compatibility, safety and pedestrian activity.
- B. The Zoning Ordinance requires development of activities such as shops, offices or other commercial space along the ground level of street frontage. In addition, parking structures should provide landscaping along blank walls on side streets.

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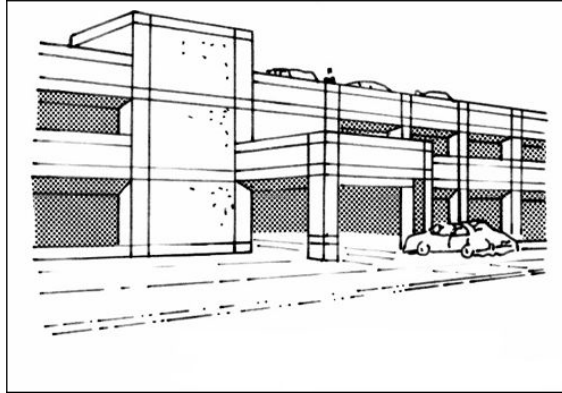
**FIGURE PD-11**  
**Pedestrian Oriented Block Face**



**FIGURE PD-12**  
**Ground Floor Retail**

- C. The following design and operational features should be considered for proposed parking structures to optimize public safety:
1. Structure design should maximize opportunities for natural surveillance into the structure;
  2. Where possible, elevators and stairs should be located on the perimeter of parking structures with natural surveillance from exterior public areas via glass back elevators and glass at stairs and elevator lobbies;
  3. Elevator lobbies and stairs in open parking garages should be open to the parking areas, except at roof levels where glass or other visually penetrable enclosures may be provided;
  4. Elevators should be provided with glass-back cabs where those elements are above grade;

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**FIGURE PD-13**

### **Entrances Shall Be Clearly Identified and Easily Accessible**

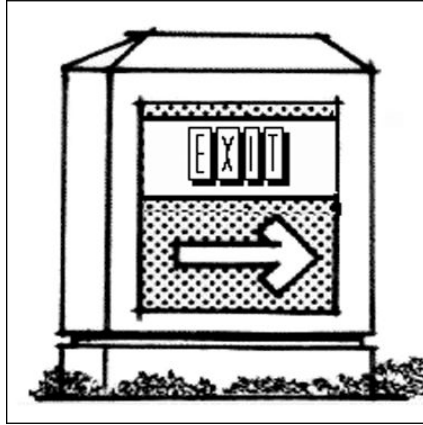
5. All parking structures should have lighting in conformance with IESNA (Illuminating Engineering Society of North America) standards;
6. Interior walls of parking structures should be painted a light color (e.g., white or light blue) to improve illumination;
7. Uniformed security guards should patrol parking structures every 30 minutes;
8. Emergency buzzers and telephones should be installed in easily accessible places on each level, in elevators and in stairwells; and
9. Directional arrows and signs indicating exits, elevators, and emergency buzzers/telephones should be clearly displayed (painted) on walls.



**FIGURE PD-14**

### **Open Lobbies and Stairs Promote Safety**

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**FIGURE PD-15**  
**Directional Arrows Should Be Obvious to Pedestrians**