

ZONING

180 Attachment 2

Town of Milton
Saratoga County, New York

Town Center
Design Guidelines
(an appendix to the Town of Milton Zoning Code)

June 25, 2001

MILTON CODE

Town of Milton
Saratoga County, New York

Town Center Design Guidelines

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TOWN CENTER DESIGN GUIDELINES

The illustrated design guidelines help implement the Town Center Master Plan, and are attached as an appendix to the Milton Zoning Code. The intent of the Town Center Design Guidelines is to provide an overall set of traditional design principles for the development of the town center. These principles are translated into specific, detailed, architectural and site design guidelines which govern the physical design and layout of the area.

The design guidelines were developed to be used by interested property owners, developers, and the public officials responsible for reviewing projects in the town center area. The design guidelines are written to be compatible with the town zoning code which along with the Land Use Element of the Comprehensive Plan shall be referred to as necessary and appropriate.

Applicability

New Construction

It is expected that the guidelines will be used to assist in the design and review of new construction projects, special permits, and site plan reviews in the town center area. In these cases, adherence to the guidelines is expected to the maximum extent practicable. Practical difficulties or potential conflicts can be discussed with town staff with the intent that solutions reached be brought to the appropriate review board for concurrence. The intent of the guidelines is to provide substantive direction while providing reasonable flexibility in their application.

Existing Sites and Structures

The guidelines apply to projects involving modifications to existing sites and structures. Where a change of building occupancy from one use to another similar use occurs, and no physical change is made, the guidelines are not applicable. Above the "substantial alteration" threshold defined in zoning, maximum compliance will be required. Below this threshold, the planning board's intent will be to move toward compliance to the degree practical.

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Over time, as the guidelines are used, it would be appropriate to review the zoning code, and make adjustments in the code, as well as to the design review process.

Principles

Successful places and memorable communities evolve from a set of guiding principles. These principles direct how the place or community will grow and develop. Guiding principles allow these areas to evolve over time and adapt to changing situations. The Town of Milton, Town Center, is no different.

The guiding principles which serve as the foundation for the Town Center Master Plan are derived from traditional principles of design. Locally, these principles are expressed in the layout and architecture of the Village of Ballston Spa and Milton's hamlets, and the City of Saratoga Springs. Regionally there are other successful examples, the Villages of Rhinebeck, Millbrook, and Chatham, New York and Northampton, Massachusetts, and Burlington, Vermont. In all of these examples, a set of principles have been applied, tested over time and remain to guide the continued use, growth, and success of these areas.



Successful communities, like Saratoga Springs, utilize principles to direct growth.

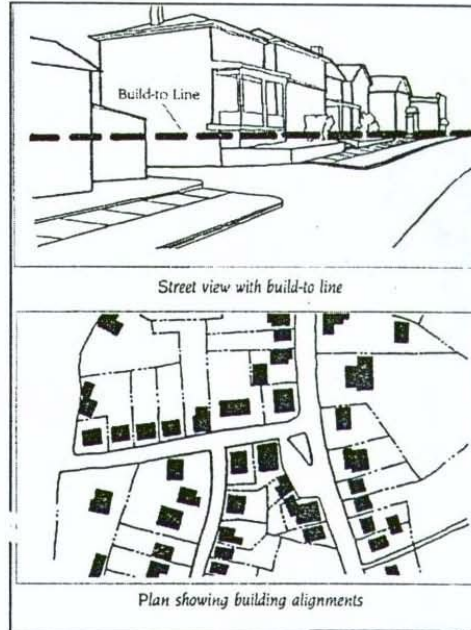
These guiding principles and their associated design guidelines are intended to support the evolution of the town center as a distinct and vital community. It should be noted however, the intent of the design guidelines is not to create a second village, rather to apply traditional design principles; to make the town center an equally vibrant and diverse community. The principles are as follows.

Apply traditional design patterns which respect area and building scale, patterns, and details

Encourage the application of traditional design patterns in new development projects within the town center area. Such patterns include:

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- traditional block-grid interconnected street network;
- walkable pedestrian sized blocks rather than super blocks;
- on-street parking;
- garages and parking lots at the rear of structures;
- sidewalks with street trees;
- buildings located close to the street with a consistent “build-to” line maintaining a consistent streetwall;
- buildings which address the public realm at street level (e.g. front doors on front walks, engaging window displays and treatments, etc...);
- buildings which respect the size and scale of existing structures in the area; and the use of building materials, such as brick, wood, and wood shingle siding, which are consistent with the overall character of the greater Milton area (this includes the Village of Ballston Spa and area hamlets).



Relate buildings and sites to the main street frontage along Rowland Street and Geyser Road

- All new buildings and substantial alterations shall be organized in such a way that the front of the structure addresses the public realm which exists along the streets and be located along the building line close to the sidewalk.
- Area setbacks shall be consistently applied throughout the town center



This corner building relates well to the road frontage through its orientation, architectural treatments, and inviting streetscape. Notice parallel parked car on left side of photo.

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area and shall be sufficient to provide for a mix of public, semi-public, and private spaces and uses.

- Provisions for public access such as, sidewalks, trails, and building entry areas, shall be incorporated into the site design of all town center development which front area streets. Such provisions shall allow for pedestrian corridors and other design elements such as the construction of sidewalks, impervious paths or trails, plantings, benches, trash receptacles and bike racks, and other design elements as appropriate and defined by the design guidelines.

Create a Continuity of Interest

A continuity of interest can be achieved through the siting and organization of buildings and site elements, public amenities, and variety of land uses. Key to establishing a continuity of interest is the requirement that all new development and substantial alterations in the town center contribute to a strong unified and memorable identity. Therefore,

- new construction and substantial alterations shall conform to a general design vocabulary of styles and treatments as described in the architectural issues section, and
- the scale of alterations and new building footprints shall be reflective of, and designed to, human scale (human scale refers to the human body's proportions and dimensions in relation to its surroundings).

In addition,

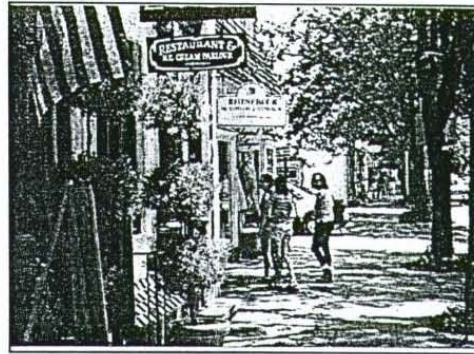
- The siting of new construction, or substantial alterations shall reflect and reinforce the building line and be compatible with existing mass and height ratios.
- Parks and other open spaces, pedestrian access, and trails shall be incorporated within any new development.



A new addition to the City of Saratoga Springs, the Roohan Building contributes well to the City's identity and is a good example of appropriate new construction.

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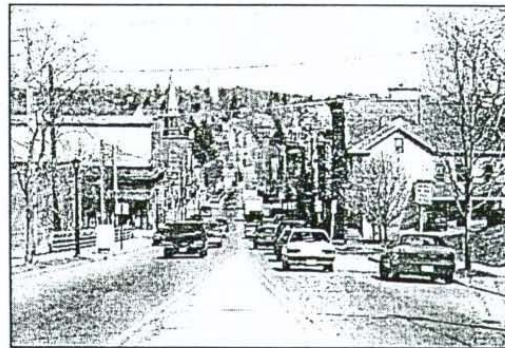
- Site elements, such as parking lots and other service entries, storage, maintenance, loading, and refuse collection areas shall be regulated to the rear or sides of buildings away from public view.
- Service areas shall not disrupt vehicular and pedestrian circulation and shall be screened from view either by the use of vegetation, appropriate fencing, a combination of the two, or through site layout and building configuration.
- The visual impact of parking shall be minimized.



The building's relationship to the sidewalk, signage, planters, flags and banners, interesting window displays, and street trees contribute to the human scale and lively atmosphere of this area.

Use Good Local Examples of the Vernacular

Attractive older buildings and public spaces within the Village of Ballston Spa and area hamlets should serve as models for new construction, alterations, and the design of public spaces. This local vernacular, as expressed through site and architectural details such as: building height and roof design, building scale, proportion and materials, facade composition and fenestration, and appropriate landscape elements, shall be incorporated within the design of new buildings, alterations, and public spaces. Overall, the design of new construction, as well as infill construction, or alterations, shall enhance the character of the town center area and contribute to, not detract from, a unified and memorable identity.



The Village of Ballston Spa has several good examples of appropriate building siting and architectural styles which could be applied to the siting and design of buildings and public spaces in the Town Center.

Utilize Ideas from Other Communities

Look toward other successful communities for examples of development which highlight quality, aesthetics, and safety and incorporate those elements, deemed most compatible, into the development of the town center area. In addition to the communities highlighted in the master plan, much can be learned by examining other traditional design patterns in communities which border the greater Milton area.

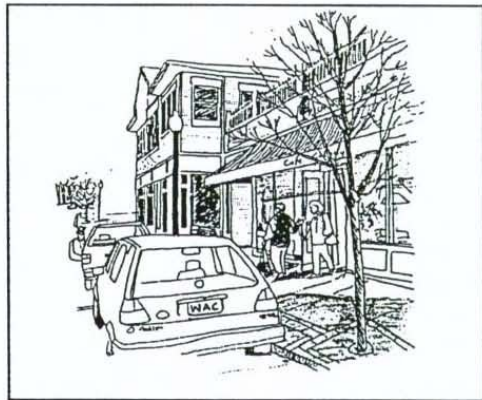


A unique blend of mixed use shops, businesses and residences, Manchester Vermont is an example of a well planned and designed community.

Create clear vehicular movement and pedestrian patterns

The Town Center shall be designed to accommodate both pedestrians and vehicles.

- Streets shall form an interconnected network to distribute automobile traffic while providing an attractive and safe environment for pedestrians.
- Sidewalks shall be an integral component of the transportation infrastructure in the town center, as shall bike and/or multi-purpose trails.
- The front yard setback in the Town Center zoning district shall be a “reserve zone” for the provision of sidewalks and streetscape improvements.
- Access to community based transportation shall also be provided in an attractive and safe location.
- Vehicular and pedestrian movement shall be separated through the installation of a pedestrian friendly streetscape. In areas where vehicular and pedestrian conflicts occur, or may occur, pedestrian movements shall be upgraded to an equal or greater



Mashpee Commons, a new town center in Cape Cod, Massachusetts, utilizes a pedestrian friendly streetscape to clearly separate pedestrian and vehicular movement.

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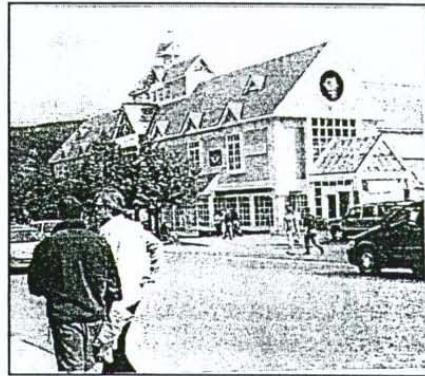
- priority than vehicle movements.
- Vehicular and pedestrian safety can be increased through the use of traffic calming mechanisms and confusion decreased through adequate and clear signage.

Architectural Issues

1. Height, mass and siting

Appropriate siting and incorporation of visual elements will establish a strong unified and memorable town center area reflective of traditional design patterns. Generally, the mass and height of new buildings shall be reflective of the mass and heights of adjacent buildings within the town center.

- New buildings shall not compete with, nor destroy the existing street line and skyline but shall enhance its overall character.
- The height of new construction in the town center, shall correspond to the height limits listed in the town's zoning code for the town center.
- As the town center develops, building massing shall be compatible with that listed in the town center zoning.
- Parking lots, and other service entries, storage, maintenance, loading, and refuse collection areas shall be regulated to the rear or sides of buildings and screened from view through site layout, building design and configuration, and where additional screening is needed, by the use of vegetation, appropriate fencing, or a combination of the two.



It is important to require new development and alterations to reflect the desired architectural character. This building reflects the architectural character desired for the Town Center. The building's pictured below do not and shall not be accepted in Site Plan review.



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According to the Town of Milton Zoning regulations, building height is limited to a maximum of 3 stories or 50 feet in the town center area. In addition the architectural details of new buildings, such as the roof line, building facade, window selection, fenestration, etc., shall remain sympathetic to the buildings in the vicinity and respond to the local architectural styles. New development shall strive to blend into the overall town center context. Where possible new buildings shall also provide strategic openings in building lines to access important vistas and public spaces.

Alterations to Existing Sites and Structures

Alterations to existing buildings are permissible in the town center area. Substantial alterations (as defined in zoning) must comply with these guidelines. All requests to alter existing buildings must be reviewed by the planning board and permission granted before undertaking construction.



The community of Freeport, Maine, worked with the developer of this McDonald's to make it fit the character of the community--Milton can do this too.

Alterations are allowed in front of the Rowland Street or Geysers Road facade of the principal existing structure. However, no alteration shall be closer to the street than the minimum front yard setback of ten feet, (10'), nor farther than the maximum front yard setback of fifteen feet, (15'), as defined in the town zoning code. Furthermore, the siting of alterations shall reflect and reinforce area setbacks and building line continuity and be compatible with town center zoning mass, bulk, and height ratios.

- Buildings, alterations and plantings shall form a definitive edge to the street instead of a dominance of pavement and parking lots.
- Alterations to existing buildings shall maintain and/or improve existing or build new street front entrances which engage the public realm.
- Where existing buildings do not related to the street, the front yard setback shall contain at minimum either a low fence, low hedges, an assortment of plantings and/or a combination of such items.

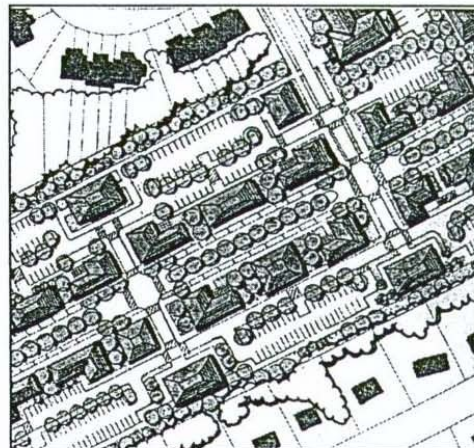
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- Additional new parking areas shall be regulated to the rear or sides of buildings away from public view.
- Alterations shall be complementary to the existing buildings architectural design and shall not distract from the overall architectural theme of the town center area. It is not necessary for alterations to repeat exact building details or architectural features, however, a uniform architectural style shall be strived for.
- Alterations shall not be significantly more articulated or more elaborately detailed than the original structure.

New Development and Construction

The siting of new development shall be respectful of existing viewsheds, natural features, and the integrity of the site's character as expressed through its landform and landscape. The physical location and logical organization of buildings, in relation to both the surrounding natural and built environment, shall be considered in the siting of new development. In addition,

- Lines of sight shall end on important visual elements such as significant buildings, or public spaces, never end on a blank wall.
- All new buildings shall address area streets and be located no closer to the street than the minimum front yard setback of ten feet, (10'), nor further than the maximum front yard setback of fifteen feet, (15'), as defined in the town zoning code. Furthermore, the siting of new buildings shall reflect and reinforce area setbacks and building line continuity and be compatible with town center zoning mass, bulk, and height ratios.
- The primary building entry shall face the street and a secondary entry shall face the parking lot to provide access for patrons, employees and deliveries. The creation of additional side and back entrances to buildings will render side and rear parking lots



The Town Center Master Plan illustrates how buildings can be placed as a terminus to significant lines of sight.

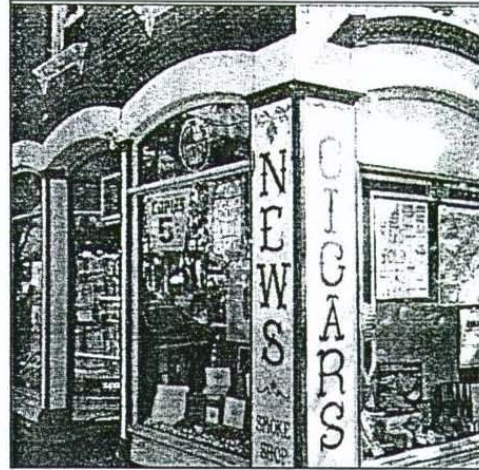
more attractive to customers and the buildings more visually interesting to pedestrians.

- The design of new construction shall be compatible with (materials and pedestrian elements, building massing, roof slopes, proportion and size of openings, and architectural details, etc.) and positively contribute to the overall organization and architectural theme of the town center area and not detract from it.

- Buildings which face the street shall relate to the street through an interesting facade, entranceway, window treatment or scheme and never a blank wall.

- Open space, courtyards, and landscaped pathways shall be encouraged to provide transitional areas between public and private spaces.

- Major buildings shall be placed as a terminus to the intersection of Geysler Road and Rowland Street.
- Minor buildings shall serve as connectors between the main intersection of the district and its outer edges. These buildings shall be located along the frontage of Geysler Road and Rowland Street.
- Where feasible, several smaller infill buildings shall be placed along, and relate to, the district's internal streets.
- Larger buildings shall be broken into smaller more human scaled structures.
- Buildings shall be grouped to the extent possible to encourage higher density development.
- Connections between uses shall be encouraged and considered when siting adjacent buildings in the town center area.
- Corner buildings shall be designed to wrap the corner by continuing design elements like horizontal bands or cornices.
- New construction shall feature levels of articulation similar to those of principal existing structures.
- New construction shall feature elements such as open or enclosed porches and landscape amenities which are at a human scale.



An example of a building wrapping the corner. Notice the large windows and distinctive signage.

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- Building groupings are encouraged.
- Site plan approval is required for all new construction projects within the town center area prior to construction.

2. Roof Lines

Since virtually all existing principal structures in the town center area feature pitched roofs, alterations and new construction shall avoid flat or very low pitch roofs.

3. Materials

Finish materials and colors on alterations shall match or complement those original to the existing buildings. New construction finish materials and colors shall harmonize with the materials and colors of the existing buildings and positively contribute to the overall theme of the town center area.

4. HVAC Units and Dumpsters

HVAC Units, Dumpsters and other mechanical equipment shall be regulated to the rear or sides of buildings and shall not be visible or minimally visible, from Rowland Street and Geysers Road and not affect adjoining (rear) sites or uses. These appliances shall be screened from view either by landscaping and architectural treatments coordinated with the project design palette.

Site Issues

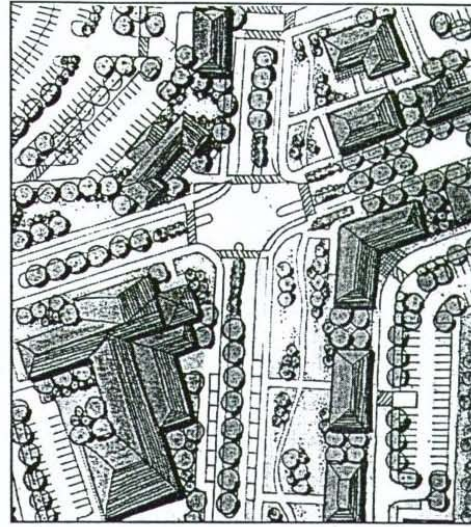
1. Transportation and Mobility

Rowland Street and Geysers Road play an important role in the transportation network of the town center area. Both streets are major transportation corridors as well as collectors for local town center business and residents. The town center master plan recognizes the need to maintain existing levels of service along the Rowland-Geysers

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transportation corridor while providing for the safe and efficient movement of both pedestrians and vehicles into and out of the town center area.

Clear patterns of vehicular and pedestrian movement are essential to creating a safe and pedestrian friendly environment in the town center area. To ensure the safe, efficient movement of pedestrians and vehicles, both modes of transportation shall be separated through the use of a well defined streetscape, the use of traffic calming mechanisms and road improvements. Pedestrian and Streetscape elements and Traffic calming mechanisms are discussed in Section 3—Streetscape and Traffic Calming.



The Town Center Master Plan illustrates several roadway and streetscape improvements to Rowland Street and Geysers Road.

Improvements to Rowland Street and Geysers Road in the town center should include:

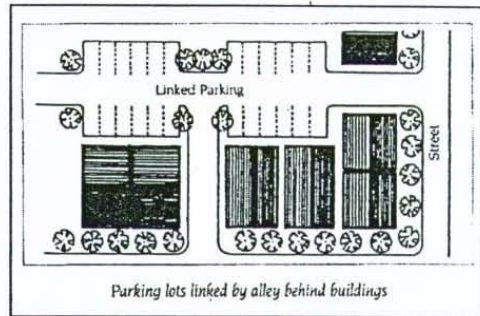
- Transformation of these roads to a tree-lined boulevard with planted medians of sufficient width, eight to twenty-one feet (8'-21'), and presence to separate the flow of traffic;
- Incorporation of left turn lanes at the intersection of Rowland Street and Geysers Road.
- Average travel lane widths and left turning lane widths, shall be of sufficient dimension, ten feet (10') minimum, to safely accommodate the majority of automobiles and single axle trucks.
- Turning radii at the Rowland-Geysers intersection shall also be of sufficient dimension, to safely accommodate the majority of automobiles and single axle trucks;
- Consolidation of entrances and exits to area establishments and reduction in the number of turns and their associated curb cuts;
- Clear uniform and safe intersection alignments;
- Incorporation of traffic calming mechanisms, and;

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- Creation of secondary access lanes to area establishments.

In the town center area, secondary, two lane access shall be developed to facilitate access to area establishments for customers, residents, service and emergency vehicles. Interior roads shall be developed according to the following criteria:

- Secondary access lanes shall be of sufficient dimension, for one lane of travel nine feet (9') for two lanes of travel eighteen feet (18'), to safely accommodate the majority of automobiles and single axle trucks, and;
- Intersection turning radii shall also be of sufficient dimension, to safely accommodate the majority of automobiles and single axle trucks.



An example of shared access and parking which shall be utilized in the town center area to maximize development potential.

2. Linkage and Curb Cuts

The clear and orderly organization and movement of pedestrians and vehicles would be further enhanced by linking destination places together via a pedestrian network and by the use of curbs to reinforce the separation of vehicular and pedestrian areas.

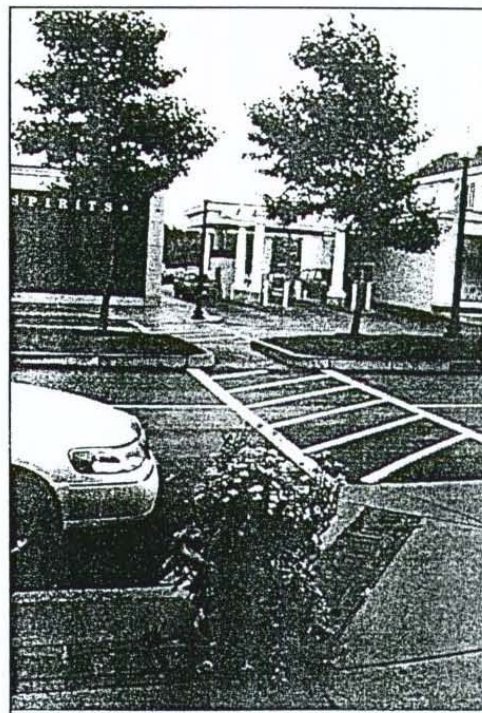
- Eliminate or reduce excessive curb cuts;
- Encourage establishments to share parking areas, common entrances and exits, and to provide sidewalk connections between them;
- Install curbs to define the edge between parking areas and roadways;
- Provide sidewalk connections between stores and parking areas;
- Incorporate public access for pedestrian and bicycle circulation, especially if the project abuts existing trails or public access areas;
- Link the town center area to other residential areas, commercial areas, recreational centers, and trail networks through the use of sidewalks, bike lanes, or off-road multi-use trails.

3. Streetscape and Traffic Calming

Safe and efficient movement of pedestrians can be accomplished through a well defined streetscape. A well defined streetscape creates an attractive pedestrian-oriented environment with clearly defined pedestrian, vehicular, and shared or overlapping zones. Traffic calming mechanisms are often used hand-in-hand with streetscape and road improvements. Together these improvements introduce visual elements, along the road edge, which alert drivers to increased activity and the presence of pedestrian traffic.

In areas of shared movement, traffic calming mechanism are used to manage the overlap between pedestrian and vehicular zones. These shared areas occur most frequently along road shoulders, at intersections and crossings, and in parking lots. Traffic calming elements include:

- The realignment of dangerous intersections for clear visual observation;
- Incorporate curves into street design;
- The narrowing of travel lanes;
- Changes in pavement materials, texture and color;
- Construction of sidewalks and curb bump outs;
- Incorporation of pedestrian cross-walks and speed bumps;
- Use of planted medians and street trees;
- Use of appropriate signage, pedestrian crossing signals and traffic lights;
- On-street parking;
- Buildings set close to the road, and;
- Clear boundary or transition zones which define and mark road edges.



Elements of a pedestrian friendly environment include curb bump outs, cross walks, planted medians and sidewalks.

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The town center area would benefit from the development of a road improvement and streetscape improvement program. Guidelines for the streetscapes within the town center include:

- Incorporation of crosswalks at high pedestrian and vehicular conflict areas. Crosswalks shall be highlighted by constructing them in a different material than the pavement or shall be striped prominently;
- Installation of appropriate traffic signals and devices such as pedestrian crossing signals;
- Installation of appropriate signage (see Section 9 Signs, Banners, and Awnings for types);
- Planting of street trees at sufficient intervals;
- Installation of human scaled lighting fixtures at sufficient intervals
- Planting and maintenance of a grassed areas of adequate width, to separate the pedestrian sidewalk from the vehicle travel lane or on street parking lane and accommodate tree plantings, lighting, signage and snow storage.
- Sidewalk corridors shall relate to building and store front entrances, link streets with parking lots and provide for safe efficient pedestrian movement.
- Human scaled streetscape elements such as sidewalks, street trees, benches, bike racks, trash receptacles, and planters shall be incorporated.
- Sidewalks shall be built to town construction standards and incorporate universal access standards for handicapped individuals where appropriate and necessary.
- The importance of the Rowland-Geyser intersection shall be recognized and promoted as the center point of the town center area. Transitions between the town center area and adjacent lands shall also be clearly demarcated as point of arrival, or departure. Special streetscape treatments such as a pedestrian courtyards, benches, directional signage, light fixtures, banners, and decorative paving materials shall be utilized to enhance these areas.

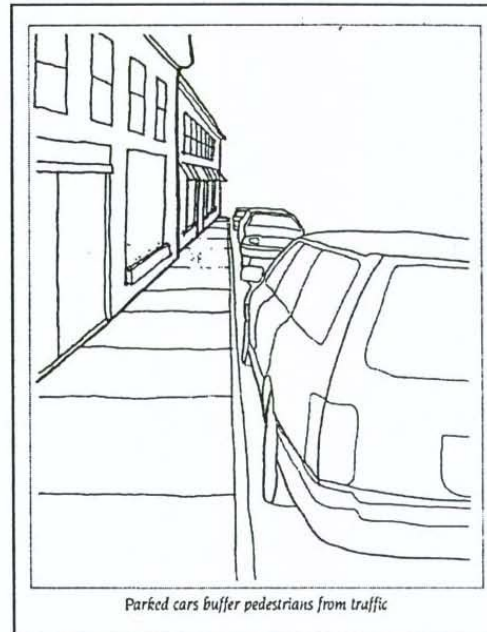


The intersection of Rowland Street and Geyser Road, the heart of the town center, shall be celebrated and given a festive atmosphere.

4. Parking and Circulation

The design of parking lots shall provide for the safe and efficient movement of both pedestrians and vehicles.

- Shared parking facilities should be encouraged to the maximum extent possible.
- Larger parking lots shall incorporate elements such as islands with plantings to break up the mass and space of the parking lot and to provide an area for safe pedestrian navigation.
- Secondary access lanes shall be designed to link and unify the uses in a project.
- Secondary access lanes within a new development shall include the amenities associated with a human scale environment, such as curbing, trees, sidewalks, and lighting.
- Parallel on-street parking along Rowland and Geyser shall be provided, where feasible.
- Parking requirements, as addressed in the town zoning code and specific to the town center area, shall be adhered to for all new development and construction projects.



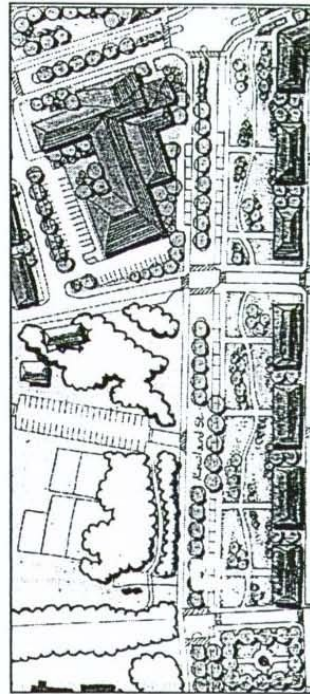
The separation of pedestrians and vehicles will ensure contribute to the safe pedestrian friendly atmosphere of the town center.

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5. Transit and Transportation Management

Appropriate alternative modes of transportation shall be identified and implemented to reduce the number of automobile trips in and through the greater Milton area.

- Develop a dialog between the town, county and the Capital District Transportation Authority (CDTA) about expanding bus service to the greater Milton area.
- Construct sidewalks and multi-use trails to accommodate high peaks of pedestrian and non-motorized movement.
- Improve the existing park-and-ride lot, located at the town offices, or relocate it to a location in the town center area which has adequate access and parking.
- Incorporate provisions for bus shelters and other commuter services into the development of the town center area.



The Town Center Master Plan has incorporated opportunities for trail access between the town center and the town park.

6. Landscaping and Street trees

The Town Center area's character shall be enhanced through the use of landscaping as part of its streetscape and road improvement programs. It has been demonstrated that pedestrian-oriented environments, which include the use of landscaping as part of a streetscape improvement program, generate more activity and interest along the road frontage. Therefore, all landscaping and landscape treatments shall enhance the pedestrian experience in the Town Center area.

The Town Center area's landscaping shall include street trees, planters, buffer plantings, parking island plantings, as well as, individual establishment's landscapes which are designed in harmony with the area's overall theme. Trees, shrubs, flowers, and ground covers which are visually appealing throughout the seasons and hardy for the area's climate shall be used as appropriate.

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Street trees are recommended in the town center area for several reasons. First, they shade and enclose the street and pedestrian areas. Second, because of their vertical nature, they act as walls further defining the transition zone between vehicular and pedestrian corridors of movement. Third, by virtue of their location along the street and their vertical nature they reduce the vast physical space which exists between building setbacks on either side of the street. Last, by reducing the physical space, smaller more intimate areas of space are created. These smaller areas of space are more pedestrian friendly because they are smaller in scale and relate better to human dimensions. Therefore:

- All town center streets shall be lined with street trees. This includes secondary access lanes as well as those portions of Rowland Street and Geyser Road within the town center area.
- A reserve area, of adequate width, shall be designated for street tree planting in all new developments.
- The removal of healthy street trees shall be discouraged and efforts shall be made to preserve existing street trees.
- Street trees which are integral with, or adjacent to, pedestrian walkways shall be limbed to a standard height which allows for a clear walking zone.
- In areas of high pedestrian traffic protective measures such as curbing to define tree planters, tree grates to allow infiltration of water and protection of roots, trunk protectors and guide-wire shall be used.

Buffer plantings shall be used to screen parking lots, utility facilities, dumpsters and other unsightly elements, as well as adjacent residential areas.

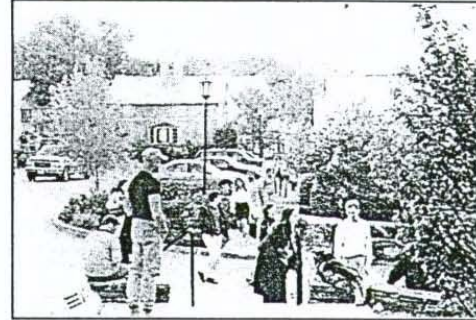
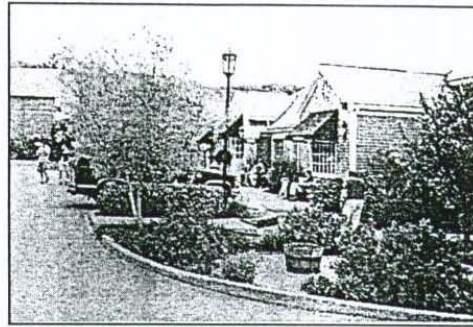
- In parking lots, large areas of asphalt shall be divided into smaller units through the use of landscaping or other techniques.
- Planting islands shall be large enough to support mature trees and snow storage.
- Trees and other plantings in parking lot islands shall be tolerant of salt, cold temperatures, and be physically strong enough to support a load of snow without additional protection.

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Overall landscaping and landscape treatments, in the town center area, shall:

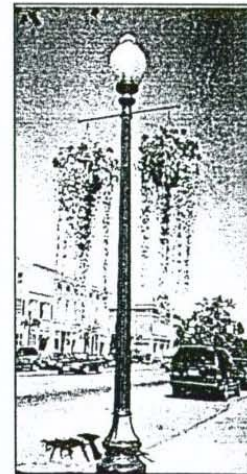
- Maintain views of vehicular and pedestrian movement corridors;
- Be visually appealing and hardy;
- Provide visual relief, shade, and;
- Buffer areas between adjoining land uses.

Suitable trees, shrubs, flowers, and ground covers shall be used in multiple combinations where feasible. Adequate spacing is also recommended to ensure high survival rates for plantings. In areas where the long term mature plantings are impractical, container plantings are encouraged, provided they are tasteful and strategically located.



An example of the use of vegetation to screen undesirable views, break-up large expanses of pavement, and contribute to the character of the development in Manchester, Vermont.

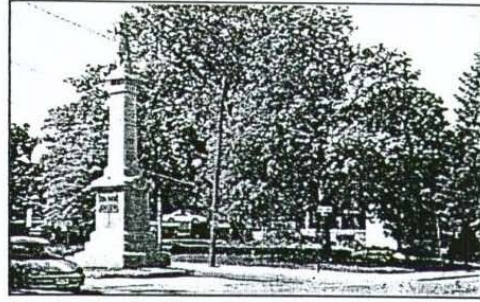
A variety of landscaping and landscape treatments are permissible for individual establishments provided they are designed in harmony with the area's overall theme. As a rule of thumb, the design shall strive for four season, co-ordinate interest of flowers, leaf color, and texture, berries, fall coloring, and branching structure. The design shall also be responsive to the three tier spatial qualities of plantings, including ground, vertical and horizontal/overhead planes.



Hanging baskets create visual interest and contribute to the character of a place.

7. Open Space and Amenities

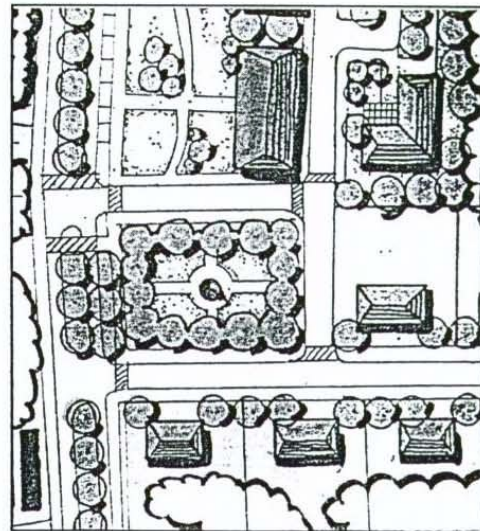
In existing commercial areas, the expansion of open space and planting areas can be used to improve the aesthetic nature of the site. In new projects, open space shall be required as an integral component of the design scheme, rather than a remnant of the development process.



Pocket parks like this one in the Village of Ballston Spa, can be used for a variety of activities.

New development and substantial alterations shall:

- give consideration to existing viewsheds and natural features.
- Incorporate parks and other open spaces, where feasible, to provide transitional areas between public and private spaces.
- Retain existing vegetation on site, where feasible, to buffer views into areas with new construction or alterations. Existing vegetation, particularly large trees, shall be inspected by a qualified arborist (tree specialist) to determine general health conditions. Trees in generally good health shall be retained and maintained while all others including those with calipers smaller than four inches (4") shall be removed. All undesirable or marginal understory vegetation shall be removed unless otherwise noted and more desirable vegetation shall be planted.
- Maintain and enhance the existing town park.
- Develop a direct, unobstructed trail connection between the existing town park trail and proposed multi-use town center trail.
- Upgrade the condition of the existing town park trail to serve as a multi-use trail.



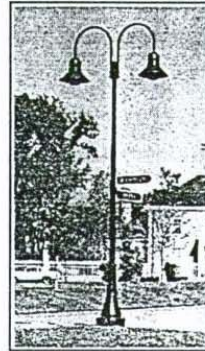
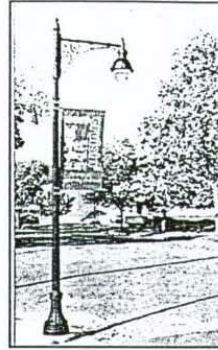
This pocket park provides a nice transition between residential neighborhood, the multi-use trail and the adjacent town park.

ZONING

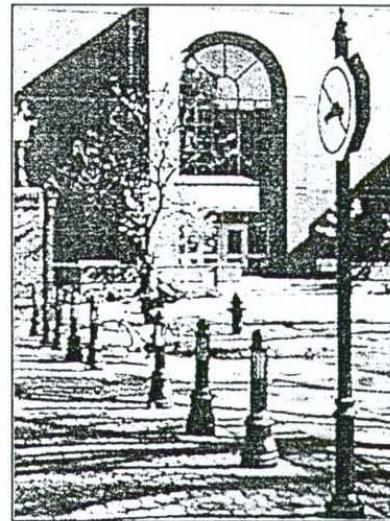
8. Lighting

Lighting is an important component to any streetscape program. Lighting provides more than just illumination, it provides security and character. A variety of lighting techniques, luminaries, and fixtures shall be used in the town center area provided they adhere to and enhance an overall lighting theme.

- Lighting fixtures shall be of adequate scale, wattage and mounting height to provide secure, attractive illumination throughout the town center area.
- Lighting in the town center area shall provide security and contribute to the character of the area. Up lighting in landscaped areas and building facades will contribute to the attractiveness and character of the area.
- Fixtures and luminaries used within the town center area shall fit the design palette of the area and be acceptable by town standards (if/when such standards and details are developed).
- Continuous illumination shall be provided along the outer and interior street network and pedestrian and trail systems.
- Lighting fixtures for parking lots shall be between 10-20 feet in height, depending on the setting.
- Metal halide lights may be used for parking lot lights.
- Parking and circulation lighting fixtures shall include a cutoff type luminary to prevent spillage of direct light above the fixture.



A variety of lighting styles exist, however, a uniform lighting style shall be used throughout the town center.



Lighted bollards shall be used to reinforce the separation between pedestrian and vehicular movement as well as to provide lighting for pedestrian and trail systems.

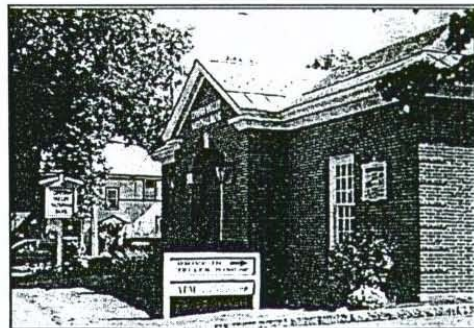
MILTON CODE

- Intensity levels of individual fixtures can be reduced by utilizing a larger number of small light poles.
- Pedestrian and trail system lighting shall be on fixtures not exceeding fifteen feet in height. Generally, light sources shall be relatively low to the ground in order to remain in scale with human dimensions and provide illumination beneath the canopy from trees. These can be freestanding fixtures located along the sidewalks. Luminaries without cutoffs are acceptable for pedestrian-level lights however, as a general rule all outdoor lighting shall be screened by shields or hoods to prevent glares onto adjacent premises.
- Incandescent lights and high pressure sodium-vapor lights shall be used in pedestrian areas, metal halide lights shall be avoided in these areas.

9. Signs, Banners and Awnings

Signage shall be clear and visible to passing pedestrians and motorists and shall be located at regular, strategic intervals throughout the town center area. A hierarchy of signs, such as: accessory, advertising, and directional, shall be developed in the town center area. Basic guidelines for signage include the following.

- Signs used within the town center area shall fit the design palette of the area and be acceptable by town standards (Dimensional standards are in the town code. These may be revised, and architectural standards may be developed, in the future).
- Signs shall be easy to read, complementary to its surroundings and not distracting.
- Freestanding signs, where permitted, shall coordinate with the building's materials, be legible, of adequate size and unobtrusive.
- Signage on buildings shall be simple, correspond to the buildings materials, integrated into the design of the building itself, and scaled down as much as possible.
- Where street numbers are used, they shall be clear, simple and placed directly above or next to the entrance door. In buildings with multiple



Simple and integrated building signage examples.

ZONING

addresses, the street numbers shall be coordinated to be of a consistent size, font and location.

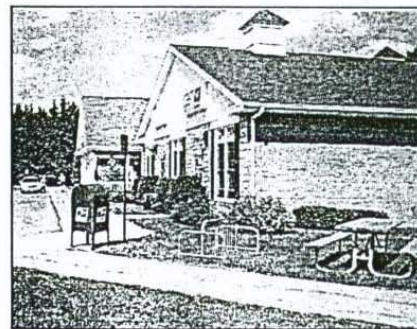
- The size of lettering used in the sign shall be in proportion to the size of the sign and the building it will be advertising, and it shall be legible.
- The painting, striping, lighting, or other graphic or attention getting device, is considered a sign and will be regulated by these guidelines.
- No structure shall be so stripped.....so as to make the structure or building a sign the sign shall be a simple device which expresses the nature of the activity occurring on the premise.
- Banners may be used as signs and shall adhere to the above guidelines. In addition, banners may be incorporated into the landscape, utilized on stand alone poles, or as part of lighting fixtures.
- Banners may be used to promote events and festivals or reflect seasonal celebrations.
- Banners shall contribute to the festive vibrant character of the town center area.
- Where signs are designated to highlight walking or multi-use trails, a centrally located directory shall be placed in a suitable area within the town center. The directory shall contain a map directing pedestrians to points of interest in the larger community and other community information.



Banners, like lighting and plantings, shall be incorporated into the streetscape of the town center.

10. Ancillary Components

Ancillary components are important additions to the streetscape. These components will play a significant role in enhancing the visual character and sense of place of the town center area. Examples of ancillary components include, but are not limited to, bicycle storage racks, water fountains, benches, tables, trash receptacles, and tree grates. These components shall be coordinated with other streetscape elements, require low maintenance, and be fairly durable.



Malta Parade Ground Village offers a nice mix of ancillary components.

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Definitions

Ancillary Components - are convenience items which include, and may not be limited to, bicycle storage racks, water/drinking fountains, benches, tables, picnic benches/tables, trash receptacles, and tree grates.

Boulevard - is a multilane transportation corridor with a central planted median and lined with street trees.

Fenestration - of and relating to the amount of depth (such as a recessed entry, or decorative feature) and openings (such as window area) on a building's facade.

Human Scale - of or, relating to, the human body's proportions and dimensions in relation to its surroundings.

Lane -

1. A public or private right-of-way which affords a secondary means of access to the lots abutting thereon.
2. A subsidiary thoroughfare providing access from within a lot, primarily from parking or loading spaces to a public street.
3. A subsidiary public thoroughfare for the sole use of pedestrians and connecting public streets, open space, or public buildings.
4. A public thoroughfare which affords only a secondary means of access for vehicular traffic to abutting lots, and which is not intended for general traffic circulation.
5. A public or private means of vehicular access to an abutting property.

Luminaries - the globe portion of a lighting fixture which protects, encases, or surrounds the light bulb and from which an area is illuminated. Luminaries may contain internal devices which cut-off the flow of light and redistribute it, such devices are generally referred to as refractors.

Median - a paved or planted area separating a street or highway into two or more lanes of opposite direction of travel.

ZONING

Public Access - relates to required provisions for trails, sidewalks, and entry areas which may, or may not, be part of private property or a public right-of-way, see also public realm.

Public Realm - is that portion of area to which the public is allowed access to private property. Typically this area includes but is not limited to provisions for public access such as trails, sidewalks, and entry areas, outdoor café, see also public access. (Insert graphic)

Vernacular - refers to the local architectural character and/or style, which is expressed by: building height and roof design, building scale, proportion and materials, facade composition and fenestration, in an areas built environment.

Turn Lane - a lane or lanes specifically designated, and separated from, the flow of vehicular traffic which are solely designated for turning purposes.