

Downtown District

Building Design Guidelines

Village of Dobbs Ferry, New York

Adopted September 28, 2010

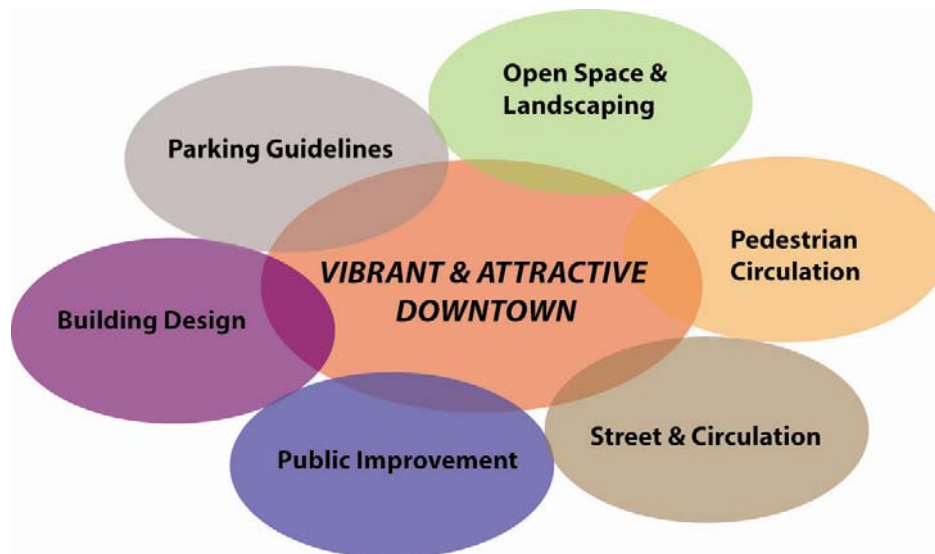
Introduction

The purpose of the Downtown District Building Design Guidelines (see Section 300-10.2, the “Design Guidelines”), is to supplement the zoning requirements of the Downtown Zones in an effort to create a vibrant and attractive downtown in the Village of Dobbs Ferry, including the area in and around the intersection of Ashford Avenue and Broadway, designated as the “Gateway”. By providing additional flexibility, with respect to the dimensional regulations and by requiring adherence to these Design Guidelines, the intent is to encourage more creative design, a more pedestrian-friendly environment and a higher quality of aesthetic and environmental performance.

These Design Guidelines are adopted by the Board of Trustees in accordance with its adoption by the Zoning Board in September 2010.

The following criteria shall be considered in the Site Plan Review and Approval of projects located within the Downtown Zone, in accordance with the information below.

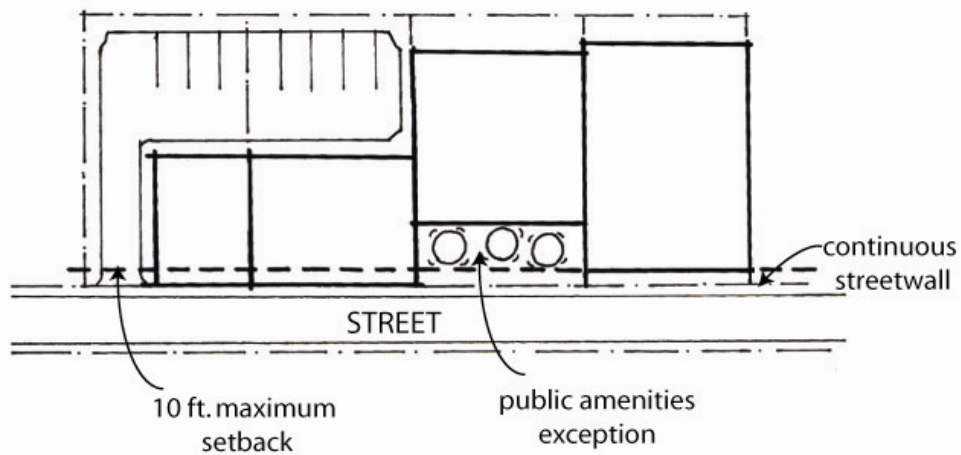
The focus of the Design Guidelines is on the relationship of the building to the street and its surroundings including the sidewalks, open space, building massing and design, and location of parking. The Design Guidelines are divided into 5 main components – Building Design, Public Improvements, Street and Circulation, Pedestrian Circulation, Open Space, Landscaping and Parking.



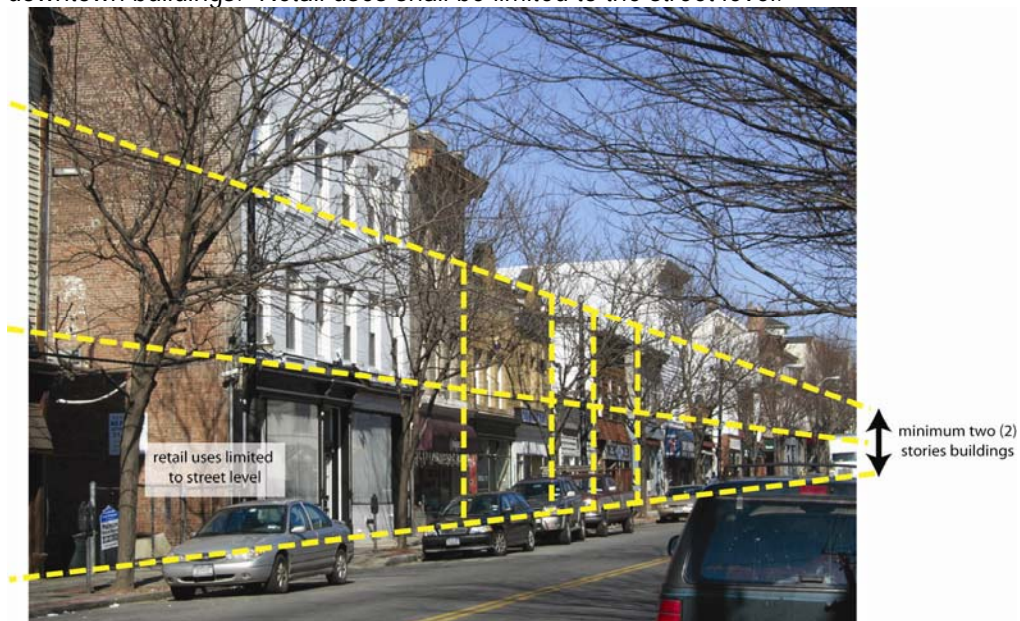
1 Building Design Guidelines

The following Guidelines shall apply to all buildings within the Downtown District:

1. All buildings having frontage on Ashford Avenue, Broadway, Cedar Street or Main Street north of the Library in the Downtown District shall contain retail; public or governmental uses at the street floor level oriented towards the street or shall have decorative facades fronting such streets. Such decorative facades shall include display windows, awnings and other decorative features giving the appearance of storefronts facing the streets.
2. All newly constructed or renovated buildings within the Gateway section of the Downtown District or having frontage on Cedar Street or Main Street shall be placed at a setback from the street right of way line so as to establish and maintain a continuous streetscape with the adjacent buildings. In no event shall the front yard setback exceed ten (10) feet.



3. All newly constructed or renovated buildings within the Gateway section of the Downtown District shall contain a minimum of two stories to complement the scale and massing of the adjacent downtown buildings. Retail uses shall be limited to the street level.



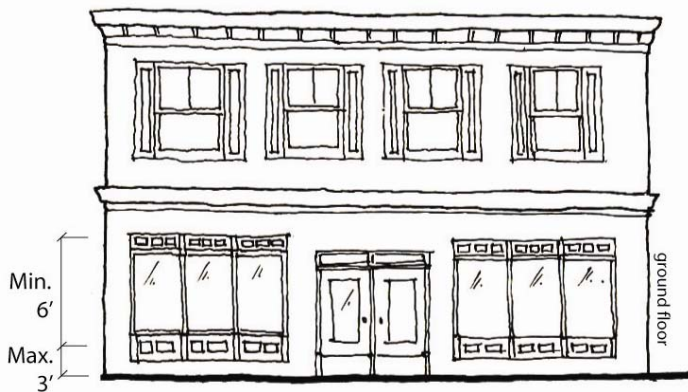
4. Along the street frontage in the Gateway section, retail stores at a scale consistent with that of Main Street are encouraged. Where larger-scale retail stores with building widths or frontages

exceeding twenty-five (25) feet are planned, architectural massing and detailing shall be used to create the appearance of multiple storefronts at the appropriate scale.



Single buildings articulated to show individual storefronts

5. No blank walls shall face Ashford Avenue, Broadway, Cedar or Main Street. On any wall facing such streets, no more than ten (10) linear feet of blank wall shall be permitted, except in the case of decorative walls screening parking. Decorative or functional display windows shall constitute at least seventy-five percent (75%) of the building width along such streets. All such windows shall be at least six (6) feet in height, with the window bottom no more than three feet above grade. Such display windows shall be of clear glazing material and provide window displays to stimulate interest and exposure of retail goods and services to the pedestrian and viewer on the street. Where public or governmental uses occupy street level spaces, windows may contain informational materials displays, artwork or other visually stimulating displays.

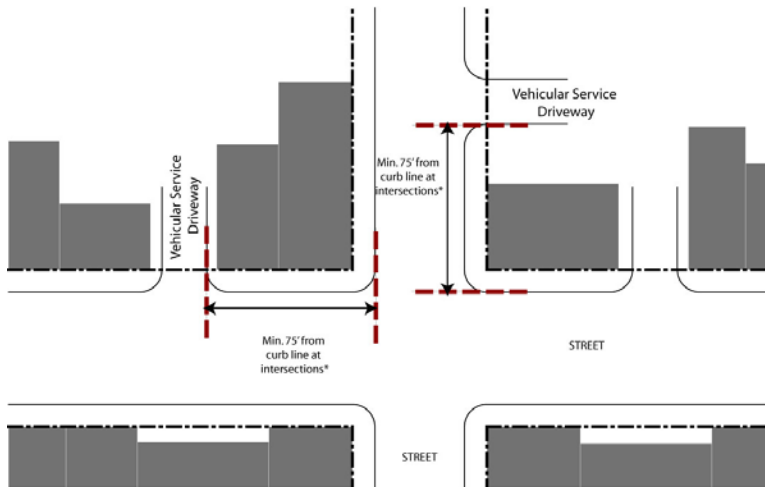


Transparency on Building Facades

6. Outdoor cafes and restaurants are encouraged in the Downtown District (and Gateway area) to increase the vitality and use of the downtown by the public during daytime and evening hours. Outdoor seating areas may extend onto the public sidewalks consistent with the provisions of the Village of Dobbs Ferry Sidewalk Café Regulations and Standards, which require a permit. Outdoor Seating areas shall be landscaped and furnished with durable, well-maintained materials. Furniture that has the appearance of being plastic or resin is not permitted.
7. All pedestrian entryways and/or lobbies are to face the street, be well lit and separate from service or vehicular entrances, and include architectural treatment that heightens their visibility.



8. To establish and maintain a continuous streetscape, no gap in retail store frontage may exceed a length of fifty feet along Main Street, Cedar Street or the Gateway section of Ashford Avenue and Broadway. Interruptions of the streetscapes, such as those required by vehicular service entrances and entrances to parking lots, shall be minimized along these streets. To improve pedestrian safety and vehicular circulation, no vehicular service driveway shall be located closer than seventy five (75) feet to the curb line of any intersection within the Gateway, except for those driveways served by curb cuts designed and installed by the NYDOT. Driveways and vehicular service entrances crossing sidewalks and pedestrian walkways shall be paved in such a way as to accentuate for pedestrians the risk of cross traffic.



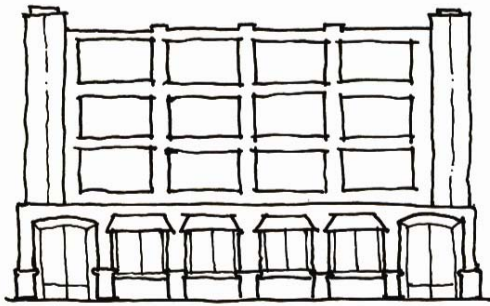
*Except those driveways served by curb cuts designed and installed by the NYDOT

Minimum Distance of Vehicular Driveways from Intersections

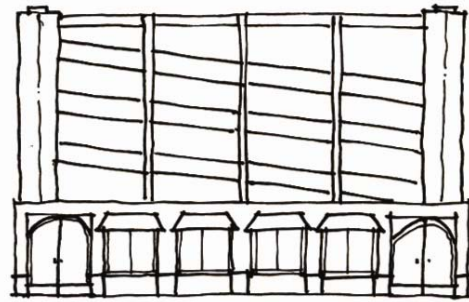
9. Where practical alternatives exist, parking should not be located in the front yard of parcels in the Downtown District. Whenever feasible, parking should be located behind retail storefronts, in side yards not projecting forward of the front wall of the building, and in rear yards. All parking areas in the Downtown District shall be suitably screened from view from all streets and public vantage points by decorative walls, fencing, and/or hedges at least four (4) feet in height. In cases where screened parking is permitted in the front yard, shade trees, benches, planted flower beds and paving material distinct from the paving material used for the sidewalk shall be used to soften the appearance of the parking area and to help separate pedestrians from parking lot traffic. Where Downtown District parking abuts a residential use, a vegetative buffer or combination of decorative wall, fence and vegetative screening at least at 10 feet deep shall be provided and maintained by the Downtown District property owner. The purpose of the buffer is to provide visual, noise, and

gasoline-related odor screening to the adjacent residential use. The buffer may not intrude into or obstruct the abutting residential property, public sidewalk, or site distance along the street.

10. Where practical, indoor parking facilities shall be integrated within the principal building and positioned so as not to dominate the massing of the building as seen from Ashford Avenue, Broadway, Cedar or Main Streets. No exterior facade of a parking garage accommodating more than 15 vehicles shall be located within fifty feet of Ashford Avenue or Broadway. All voids in the exterior facade of parking garages shall be screened so that light and vehicles are not visible from public streets or vantage points.



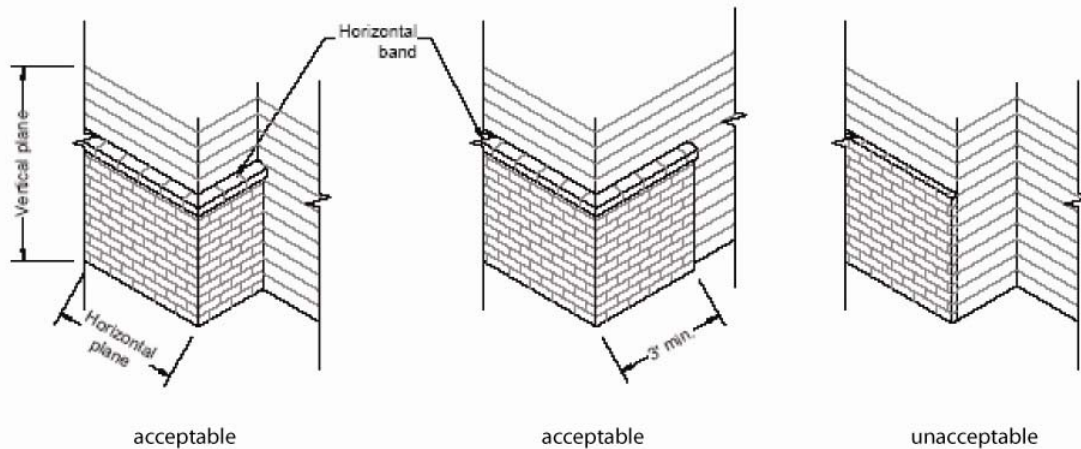
YES



NO

11. No loading or service areas shall be permitted in front or side yard unless screened from view from public streets, similar to the requirements for parking.
12. Building Facades shall complement the varied existing architectural styles, scale and massing, decorative features, window and doorway placement and proportions typified by the existing buildings fronting on Main and Cedar Streets within the Downtown District. Surface materials may include stone, brick, stucco both with and without half-timber, clapboard and shingles. Vinyl and aluminum siding are not permitted in the Downtown District. The intent is for the exterior materials, massing, and details of new and renovated buildings to complement those of existing adjacent structures and the character of the District. Windows on upper floors shall be residentially-scaled, multi-paned double-hung, casement, or awning operable windows, irrespective of the use of the upper floors. Store front fixed glass windows shall not be permitted at upper levels. Upper level windows shall not be painted over or otherwise obscured or obstructed.





13. Buildings shall provide varied rooflines and surface materials. Peaked, mansard and other sloping roof types are encouraged, while flat roofs shall be concealed by cornices or decorative parapets, so as not to be visible from the streets.
14. The scale, massing, facades and materials of all buildings when viewed from public vehicular and pedestrian circulation pathways within and around the Downtown District shall contribute positively to the streetscape.
15. Street signage and exterior lighting shall be facade mounted wherever feasible along Main Street and Cedar Streets to reduce visual clutter along those streets.
16. The interior lighting of first floor spaces facing towards streets shall be designed to avoid disturbing light pollution to the adjacent streets. Luminaries shall have integral lenses and/or shields to direct light where it is needed and to soften the appearance of the light fixture when seen from the exterior of the building. Florescent lighting tubes shall not be visible from the exterior.
17. All utility distribution lines and utility service connections to the buildings in the Downtown District shall be underground.
18. All electronic communications equipment shall be of colors and materials similar to that of the surfaces upon which they are mounted or otherwise screened from view.
19. Landscaping such as planters or window boxes, street trees and street furniture shall be provided and maintained by the property owner of parcels in the Downtown District.

2 Public Improvement Guidelines

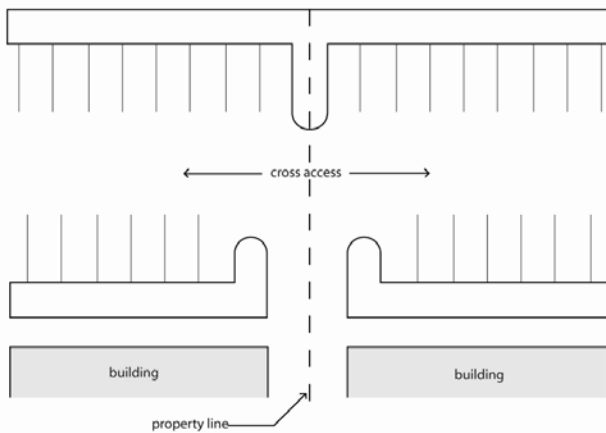
The following Guidelines shall apply to all rights-of way, both public and private, within the Downtown District:

1. Public improvements and amenities such as roads, sidewalks, bikeways and walkways, open spaces, plazas, and sheltered transit stops are integral links among the structures and uses in the Downtown District and are, therefore, to be provided for in the development and/or redevelopment of Downtown District parcels. Public/private partnerships to facilitate the development and maintenance of such public improvements are encouraged.
2. The buildings and uses in the Downtown District rely upon both public and private utilities, including water and gas services, storm and sanitary sewers, electric, telephone, and cable services. The development and/or redevelopment of individual parcels in the Downtown District shall integrate the upgrade of these utilities with the needs of the entire District. All utilities, including electric, telephone and cable services shall be buried. The elimination of overhead wires in the Downtown District is a goal of these Guidelines and each project shall provide for this objective both structurally and financially as part of the Site Plan Approval process to the extent practicable.
3. Infrastructure improvements should facilitate safe and convenient pedestrian movement throughout the Downtown District, while accommodating vehicular traffic needs and providing linkages with the adjacent road, sidewalk and bikeway network.
4. The design of sidewalks, pedestrian footpaths, signage, street lighting, landscaping and street furniture should facilitate and complement the movement of people along streets and into shops, residences, public open spaces and adjacent neighborhoods.
5. Signage should be provided in the Gateway section of the Downtown District, indicating orientation to the downtown, and directing pedestrians, passengers on public transit and motorists to destinations within the downtown as well as to the waterfront and train station. The entrances into the downtown District from the Gateway should be clearly defined and welcoming and signs should be legible to both pedestrians and vehicle traffic.
6. Public improvements shall be properly maintained and maintenance provisions may be included among the conditions of site plan approval.

3 Street and Circulation Guidelines

The following Guidelines shall apply to all rights-of way, streets, and roadways, both public and private, within the Downtown District:

1. Ashford Avenue, Broadway and Cedar Street shall be regarded as the key roadways running through the Downtown District, which also includes Main Street. Traffic movements through and within this portion of the Downtown District shall maximize the passage of through traffic, while providing direct and clearly marked access to the parking and loading areas serving the parcels in the Downtown District.
2. Minimizing curb cuts on Ashford Avenue, Broadway, Cedar Street and Main Street is an objective. Shared off street parking and loading areas will be considered where appropriate to meet this objective. The minimum number of curb cuts necessary to serve the vehicular access needs of a particular property or group of properties fronting on Ashford Avenue, Broadway, Cedar Street and Main Street shall be provided.



3. Curb cuts on the Gateway portions of Ashford Avenue, Broadway and Cedar Street shall be separated from one another on the same side of the street to the maximum extent possible, and shall be located directly opposite one another on opposite sides of the street to the maximum extent practical. Existing curb cuts that are no longer necessary to provide access shall be eliminated and replaced with curbing to match the adjacent curbing.



4. To reduce curb cuts, the resulting disturbances to vehicular traffic flow on Ashford Avenue, Broadway, Cedar Street and Main Street, which are the primary connection streets, and to facilitate improved vehicular circulation in the Downtown District, the use of the streets connecting between Broadway and Main Street, including Walnut, Chestnut, Elm and Oak, and the streets entering the Gateway, including Rochambeau, Maple, Storm, Bellewood, Walgrove, and Estherwood, shall be carefully considered in the Site Plan Review for each development and/or redevelopment project.
5. At the same time, the residential character of the side streets connecting to Ashford Avenue, Broadway, Cedar Street and Main Street shall be respected and protected. Subject to Building Design Guideline 8. above, vehicular access and service driveways serving parcels in the Downtown District shall be as close as practical to the primary collection streets. The intent is to avoid traffic unnecessarily being directed through residential areas. Signage, changes in paving materials, and other visual queues shall be used to direct traffic from the primary collector streets directly to the parking areas and vehicular service driveways serving the commercial uses in the Downtown District.
6. The land use, circulation patterns, and off-street parking areas within the Downtown District shall accommodate and encourage persons entering the Downtown District to park only once and walk to multiple destinations.
7. Where practical, accommodations for turning lanes shall be provided to facilitate vehicles entering driveways and parking lots without restricting or otherwise congesting through traffic patterns. Turning lanes shall be designed so as not to compromise pedestrian traffic.

4 Pedestrian Circulation Guidelines

The following guidelines shall apply to all pedestrian walkways, whether on public or private property:

1. A coordinated and integrated pedestrian circulation system shall be provided within the Downtown District to provide safe and convenient pedestrian circulation through the following:
 - (a) Streetscape improvements shall be focused on the primary pedestrian corridors along Ashford Avenue, Broadway, Main and Cedar Street.
 - (b) Distinctive paving materials that complement the existing paving material on sidewalks along Cedar Street, Main Street, and in the Gateway shall be used to heighten the visibility and continuity of the pedestrian walking surface.
 - (c) All curbs must be poured concrete or stone set in concrete. Stone curbing may be granite, Yonkers gneiss, or bluestone. Belgian block curbing, however, is not permitted along public streets.
 - (d) Other design features including landscaping, street furniture, lighting, signage and spaces for planned activities should be used to create a visually stimulating pedestrian environment that minimizes opportunities for vehicular conflict.
 - (e) Walkways that provide safe direct and convenient pedestrian access between parking lots and sidewalks are required.
2. Sidewalks on Ashford and Cedar Streets shall be a minimum of four feet in width and sidewalks on Broadway shall be a minimum of five feet in width. These sidewalks shall be constructed of or bordered with a distinctive paving. Walkways from public sidewalks and access ways to rear parking between shops shall be a minimum of three feet in width.
3. Pedestrian crossings of Ashford Avenue, Broadway and Cedar Street shall be provided at least once every four hundred (400) linear feet. Visibility of pedestrian crossing shall be heightened by the use of a distinctive paving material complementing the adjoining pedestrian walkways and sidewalk.
4. Integral to these Guidelines is the intent to create parking facilities to better serve the Downtown District and surrounding areas. Pedestrian walkways traversing both public and private property shall be provided to connect the parking facilities to the primary collection streets and the creation of these walkways shall be coordinated in the Site Plan Review process for each project. Public/private partnerships to facilitate the development and maintenance of such public improvements are encouraged.
5. All sidewalks and pedestrian walkways shall be provided with lighting averaging at least one-half (1/2) foot-candle across the area so lit. Lighting fixtures shall be pole-mounted above sidewalks and may also be either building mounted or bollard type along walkways. All fixtures must be shielded. Parking area lighting must provide at least three quarter (3/4) foot-candle for public safety and visibility.

5 Open Space and Landscaping Guidelines

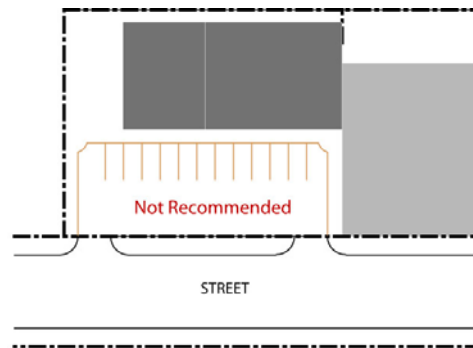
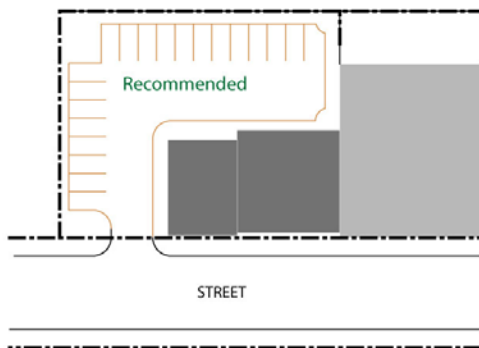
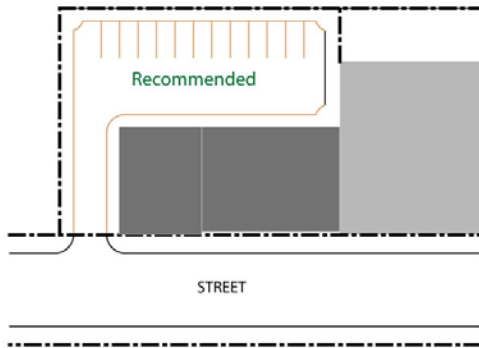
The following guidelines shall apply to open space and landscaping within the Downtown District:

1. All unpaved areas; open spaces and plazas shall be planted consistent with an approved landscaping plan, using plant materials appropriate for the location and microclimate.
2. Planting material should include trees, shrubs, hardy grasses and ground cover that minimize the need for watering and the use of pesticides and fertilizers. Watering and maintenance shall be the responsibility of the property owner.
3. Pedestrian walkways and paved surfaces used for public activities shall include the use of decorative paving materials, street furniture, lighting and other amenities to provide a street level setting that compliments the buildings and uses within the Downtown District.
4. A variety of seating types and opportunities shall be provided to meet the varied needs of pedestrians moving throughout the Downtown District, In addition to benches, seating shall include low walls, planter/seating combinations and stairs. The use of water features, such as fountains, to complement plazas and seating areas is encouraged.
5. All loading and service delivery areas, trash receptacles, HVAC units, and other mechanical equipment shall be screened by means of solid fencing supplemented with evergreen plantings tolerant of urban environments. Trash receptacles shall be fully enclosed and secured. Plantings used for screening shall be maintained at a minimum height of four feet and shall be positioned to form a continuous horizontal screen to within six (6) inches of grade.
6. Plant materials shall be installed and maintained in good condition. All deciduous trees shall be a minimum of four inches caliper at the time of planting. All plants, trees, and shrubs shall be installed in accordance with a Landscaping Plan included in the Site Plan drawings approved by the Board of Trustees.
7. Fencing materials along public rights-of-way and in front yards of parcels within the Downtown District may be of tubular steel or wrought iron type milled steel picket. Fencing within side or rear yards may be wood picket, steel picket, or any other approved fence type. No chain link fencing shall be permitted anywhere within the Downtown District, except during construction.
8. Public Plaza areas shall incorporate sheltered bus stops, bicycle racks, seating, trash receptacles, lighting, landscaping and other amenities consistent with their planned use.

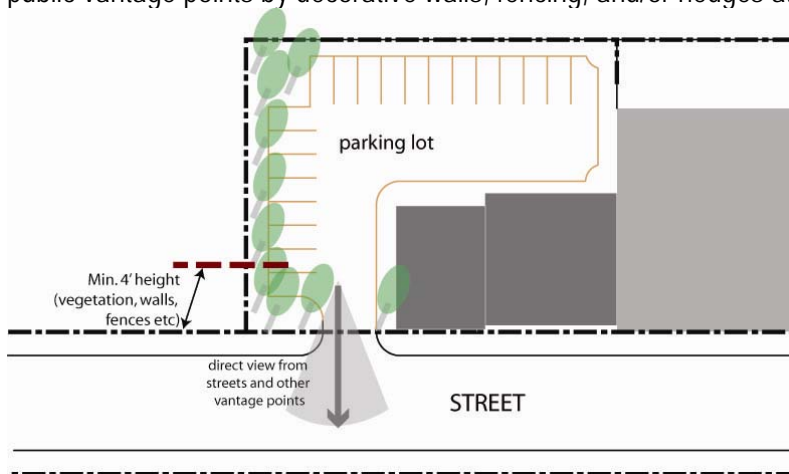
6 Parking Guidelines

The following guidelines shall apply to all public and private parking facilities, lots and areas within and serving the Downtown District:

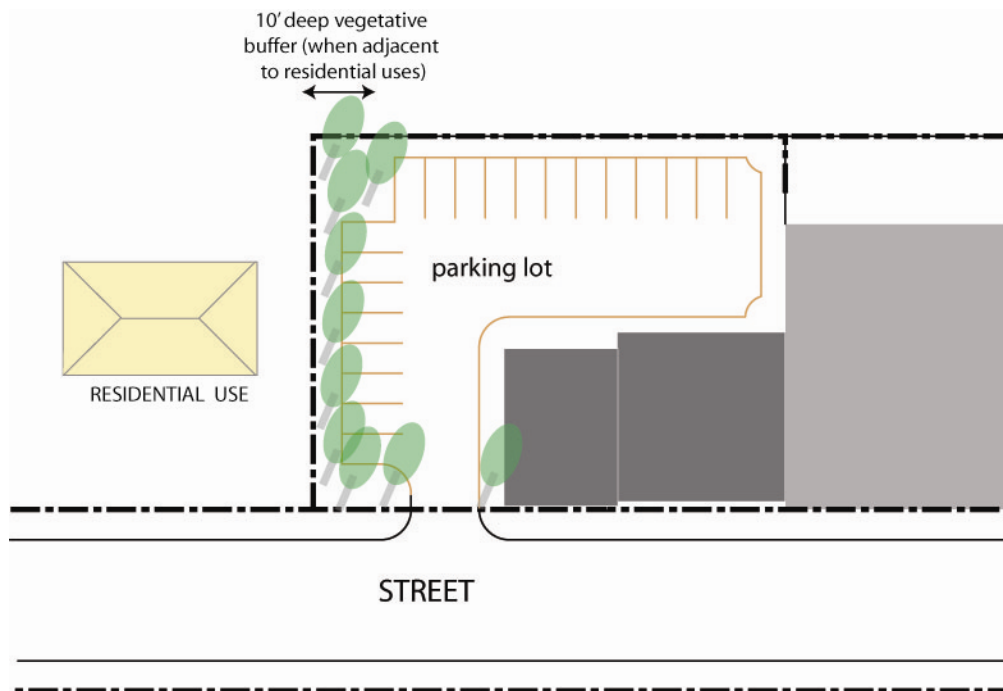
1. Where practical alternatives exist, parking should not be located in the front yard of parcels zoned "B". Whenever feasible, parking should be located behind retail storefronts, in side yards not projecting forward of the front wall of the building, and in rear yards.



2. All parking areas in the Downtown District shall be suitably screened from view from all streets and public vantage points by decorative walls, fencing, and/or hedges at least four (4) feet in height.



3. In cases where screened parking is permitted in the front yard, shade trees, benches, planted flower beds and paving material distinct from the paving material used for the sidewalk shall be used to soften the appearance of the parking area and to help separate pedestrians from parking lot traffic.
4. Parking lots with twenty or more parking spaces in total or twelve or more parking spaces in a single row shall contain landscaped islands to help direct the flow of traffic and to provide a place for shade trees to be planted. At least one tree per ten spaces shall be provided within a parking lot, in accordance with an approved landscaping plan and maintenance shall be the responsibility of the property owner.
5. Where Downtown District parking abuts a residential use, a vegetative buffer or combination of decorative wall, fence and vegetative screening at least 10 feet deep shall be provided and maintained by the Downtown District property owner. The purpose of the buffer is to provide visual, noise, and gasoline-related odor screening to the adjacent residential use. The buffer may not intrude into or obstruct the abutting residential property, public sidewalk, or site distance along the street.



6. Otherwise conforming applications for Site Plan Approval in the Downtown District for the adaptive reuse or renovation of existing buildings may be eligible for a reduction in off-street parking and loading requirements, except for applications that propose an increase in ground floor area or a loss of existing off street parking and loading spaces.
7. New development, as well as the adaptive reuse and renovation of existing buildings that does increase ground floor area or result in a loss of existing off street parking and loading space, may be eligible for a relocation of required parking through the payment of a fee in lieu of on-site parking to the Village parking fund. The intent of this fund is to create new parking facilities and maintain existing facilities.

8. Subject to Site Plan Approval, required parking for new or expanded uses in the Downtown District may be able to be satisfied by parking agreements with nearby private property owners or by the use of public parking facilities.
9. Shared use of off-street parking lots by two or more uses and joint access to off-street parking lots shall be encouraged, subject to a demonstration of the adequacy of such shared use for the existing or proposed parking demand.
10. Convenient and safe pedestrian walkways within parking lots are required.

