

ZONING

B Attachment 4

Town of Cumberland

Appendix B Development Plan Review Standards [Ord. No. 08-31, 10-7-2009]

- A. Application. In reviewing applications subject to Article 12, the Planning Board shall apply site and design standards and requirements as contained in this appendix, as appropriate to the proposed development, and the design guidelines represented in “Cumberland’s Vision Using the Past to Enrich the Future” (2005, as amended). Said document is incorporated herein by reference. Standards and regulations for signage shall be those contained in Article 14 of this appendix.

The Planning Board may grant waivers to the application of said design standards contained within this appendix, but may not grant waivers that are contrary to standards contained elsewhere in this appendix that would otherwise apply to the proposed development. Waivers shall be considered as provided for in § 3(H) of the Cumberland Land Development and Subdivision Regulations. The granting of waivers to these design standards by the Planning Board shall not be considered to be conferring the right to grant use or dimensional variances, or special use permits to the Planning Board. The Planning Board is vested with the authority to grant waivers to the design standards contained herein in recognition of the site specific nature of development review.

Upon amendment of these design standards by the Planning Board in accordance with the land development and subdivision regulations, the Cumberland Town Council shall adopt the amended design standards as appendix B to this chapter.

- B. Design. The design of all land development projects, and development plans shall conform to the Town of Cumberland Zoning Ordinance and Land Development and Subdivision Regulations. The elements contained in this appendix are to be considered minimum design standards. The Planning Board may grant waivers to these standards upon a site visit and/or review of the proposed plan, if the board feels in doing so that adequate provisions have been/must be made in the plan for the following:
- (1) To lessen traffic accidents;
 - (2) To develop land in recognition of primary natural constraints (slopes 15%, rock outcrops and wetlands);
 - (3) To promote safety from fire, flood and other dangers;
 - (4) To secure a well articulated street and highway system;
 - (5) To ensure adequate provisions for pedestrian traffic;
 - (6) To secure an adequate stormwater run-off management and soil erosion plan;

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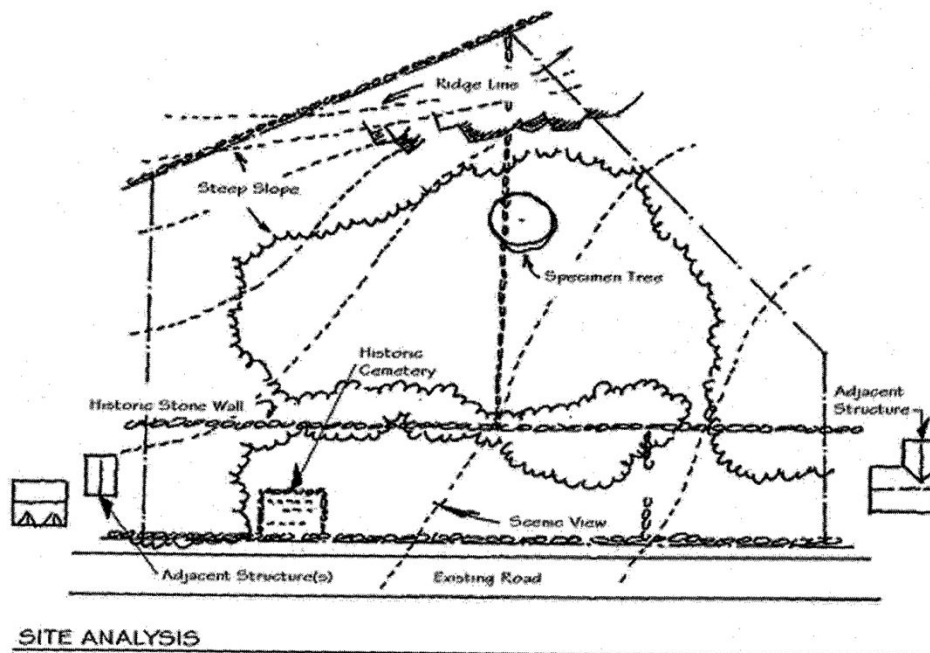
- (7) To preserve significant natural and historic characteristics;
- (8) To provide adequate public water and sanitary sewage treatment;
- (9) To provide a recreation area suitable for future use; and
- (10) To promote development in conformance with the comprehensive plan.

The applicant, at his/her own expense, shall construct all improvements where required by the Planning Board as a condition of approval for any development plan or land development project subject to these regulations.

C. Site planning.

- (1) Depending on a property's location, the styles and patterns of development are different, and therefore, the application of these standards will necessarily depend upon the surrounding area in which development is proposed.
- (2) To the maximum extent practicable, development should be located to preserve the natural features of the site, to avoid areas of environmental sensitivity, and to minimize alterations of and negative impacts to natural features, historic and cultural resources, and scenic areas. A site analysis that considers both the existing natural and built context as described below should be conducted as part of the conceptual site planning process.
- (3) New development or redevelopment shall incorporate characteristics of the surrounding area when the area exhibits a positive site layout and/or functional patterns (e.g., buildings close to street, shared parking and access, and generous landscaping); otherwise, the Planning Board will look to the applicant to improve the area with his/her proposal and not further degrade an area.

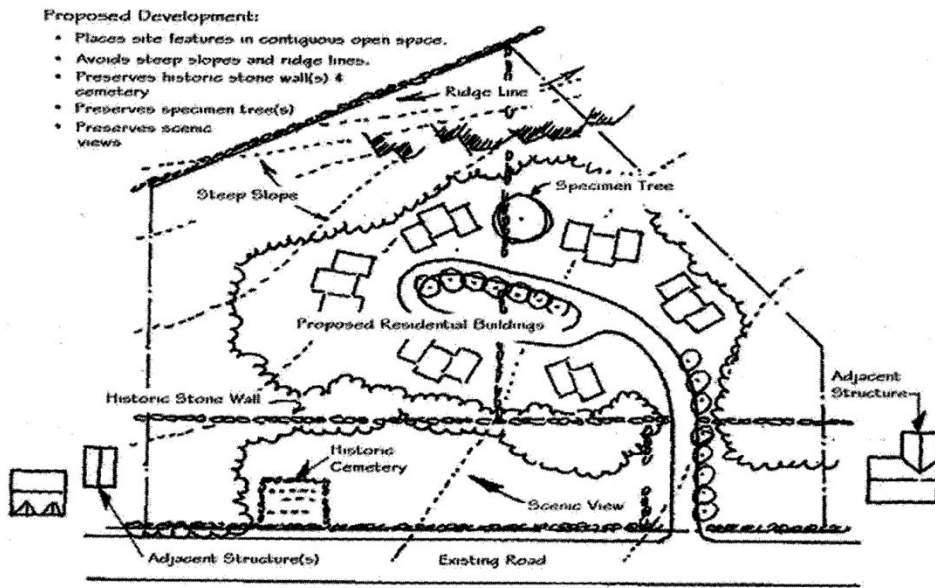
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D. Natural context.

- (1) Building envelopes shall be located so that character-defining site features such as stone walls, open fields, stands of mature trees, rolling topography (especially slopes in excess of 15%), ridgelines and outcrops, wetlands, streams, rivers, ponds and lakes, and listed historic and natural resources are preserved.
- (2) Structures shall not be placed on ridgelines or hillcrests. These areas are potentially erosive, may negatively impact drainage patterns and because they will be highly visible, will adversely impact the character of neighborhoods and scenic, natural viewsheds.
- (3) Development should take advantage of natural solar irradiation through southern exposure and design features in order to reduce energy usage and increase connections to the surrounding environment. Vegetation, berms, and shade structures should be used to provide warmer areas during winter and cooler areas during summer.
- (4) Green spaces shall be contiguous whenever possible, rather than divided into smaller areas.
- (5) Utilize the space between buildings as viable "outdoor rooms" which can function as pedestrian transition areas, provide building connections and project coherence.

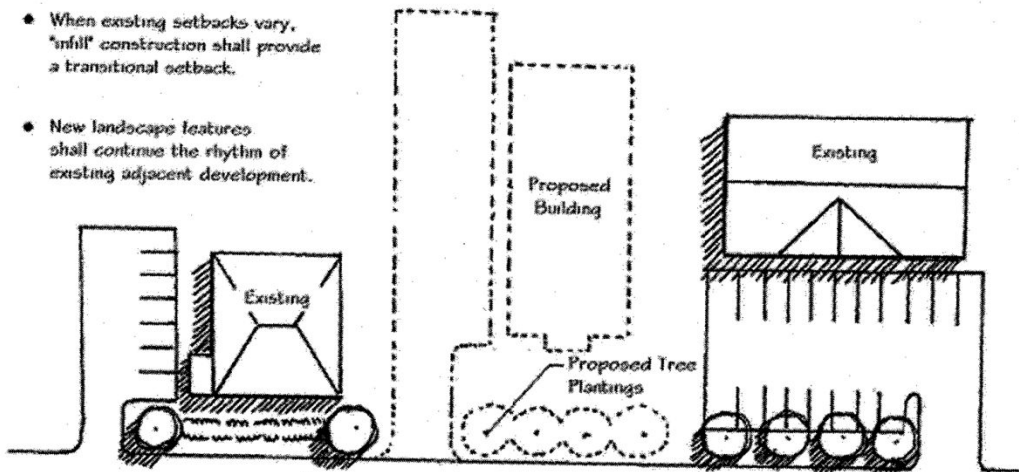
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APPLICATION OF SITE ANALYSIS TO THE PROPOSED NEW USE

E. Built context. Existing design, details such as form, type and texture of materials, balance, symmetry/asymmetry, natural factors, pedestrian circulation, access, and connections should be respected. Continuity of positive aspects of the nearby architectural style and other elements of the built environment will be the primary focus of the review process.

- (1) Placement of buildings shall consider the location of nearby compatible and incompatible uses, traffic corridors, vegetation, and other existing site characteristics. Where adjacent setbacks are inconsistent, an attempt shall be made to moderate them. If this is not possible, vegetation, walls and other landscape features shall be used to continue the rhythm of the built environment.



SAMPLE CONDITION: MODERATION OF INCONSISTANT SETBACKS

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- (2) In densely developed areas, such as the historic centers of Valley Falls, Lonsdale and Berkeley, buildings should generally be placed adjacent to the sidewalk or at their front setback lines in order to enliven the street. This siting, in combination with landscape treatment, reinforces and strengthens the streetscape and facilitates pedestrian activity.
 - (3) Multiple buildings in a single development should create a positive functional relationship. Buildings should be clustered to achieve a village feel. This creates opportunities for plazas and pedestrian areas while preventing long “barrack-like” rows of buildings. When clustering is impractical, a visual and/or landscape linkage shall be established.
 - (4) When adjacent residential and nonresidential uses can mutually profit from connection rather than separation, applicable connective elements such as walkways, common landscape areas, shared driveways, building orientation, and unfenced property lines shall be employed.
- F. Land unsuitable for development. Land deemed unsuitable for building purposes in the judgment of the Planning Board, will not be approved for development or subdivision.
- G. Developments serviced by sewers. When calculating the number of residential building lots or units permitted on any parcel in an area serviced by sewers, land included in all of the following categories shall be considered unsuitable for development and shall be deducted from the building acreage of the parcel:
- (1) Freshwater wetlands, except areas of perimeter wetland within 50 [feet] of the edge of any bog, marsh, swamp or pond; or any applicable one-hundred-foot or two-hundred-foot riverbank wetlands, as defined by G.L. § 2-1-20 (1987), as amended;
 - (2) Areas within a one-hundred-year flood zones, as defined by FEMA;
 - (3) Land within any publicly or privately held easement on which aboveground utilities, including, but not limited to, electrical transmission lines, are constructed;
 - (4) Areas with slopes in excess of 15% that are within the limit of disturbance of the subdivision; and
 - (5) Cemeteries.
- H. Developments not serviced by sewers. In areas not served by public sewers land included in all of the following categories shall be considered unsuitable for development and shall be deducted from the building acreage of the parcel:
- (1) Freshwater wetlands including areas of perimeter wetland within 50 feet of the edge of any bog, marsh, swamp or pond; or any applicable one-hundred-foot or two-hundred-foot riverbank wetlands, as defined by G.L. § 2-1-20 (1987), as amended;
 - (2) Areas within a one-hundred-year flood zone, as defined by FEMA;

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- (3) Land within any publicly or privately held easement on which aboveground utilities, including, but not limited to, electrical transmission lines, are constructed;
 - (4) Areas with slopes in excess of 15% within the limit of disturbance of the subdivision; and
 - (5) Cemeteries.
- I. Minimum contiguous buildable areas. Lots shall have minimum contiguous buildable areas (excluding wetlands, floodplains, easements, and steep slopes as further described above) equal to the following:

Zoning District	Minimum Contiguous Buildable Area (square feet)
Agricultural-1	80,000
Agricultural-2	40,000
All unsewered residential zones	20,000
Residential-1, sewerd	12,500
Residential-2 and residential-3, sewerd	5,000
All commercial and industrial, sewerd	5,000
All commercial and industrial, unsewerd	20,000

- J. Flood hazard areas. The following requirements shall apply to any development which is located wholly or partly within zone A and zones A1-A30 as identified on the flood insurance rate map as part of the flood insurance study which also includes the flood boundary and floodway map. Said maps and any amendments thereto are hereby made part of this section of these regulations.
- (1) All submissions shall show the location of any portion of the plat which lies within any zone A or zones A1-A30 and the floodway and shall show the base flood elevation as prescribed for these zones at the specific location. Where the development location is entirely within these zones, it shall be noted on the plat drawing.
 - (2) In grading land and installing improvements, no watercourse shall be altered in such a manner as to reduce its carrying capacity. Prior to permitting any alteration or relocation of a watercourse, the Planning Board will send notification to the neighboring communities, the Rhode Island Statewide Planning Program and the Federal Insurance Administration.
 - (3) All development proposals will be reviewed by the Planning Board or its agent to assure that the design of the plat is consistent with the need to minimize flood damage. Public improvements, facilities, and utilities are constructed or installed in a manner that will minimize flood damage. Adequate drainage will be provided to minimize the accumulation of water.

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- K. Erosion and sediment control design. Erosion and sediment control design shall be in accordance with Chapter 20, article III of Cumberland's Code of Ordinances, as amended.

Site design should avoid steep slopes, minimize slopes in graded areas and work with the natural drainage and topography of the site. Original boundaries, alignment and slopes of watercourses within the project locus shall be preserved to the greatest extent feasible.

Development plans should preserve natural features, keep cut and fill operations to a minimum and ensure conformity with topography so as to adequately handle the volume and velocity of surface water runoff.

- L. Access and circulation. Vehicular and pedestrian circulation should be clearly organized and functional, providing safe and efficient means of access to all non-sensitive areas of the site. Vehicular and pedestrian circulation areas should be separated to ensure safety, with appropriate linkages at designated inter-modal transportation nodes. A development's circulatory system, including roadways, paths, and parking areas provides the pattern for human experience and should be designed considering both health and safety issues as well as aesthetics, social and environmental issues.

- (1) Traffic impact study required. In any case where a drive through is proposed or a new building or new use will generate more than 20 additional trips (total of inbound and outbound) during the adjacent roadways peak hours or the development's peak hours or if it is considered that the new development may have an impact on traffic safety, a traffic impact statement may be required. The scope of the traffic study will be defined by the Planning Board. The traffic impact study shall be prepared by a Rhode Island Registered Professional Engineer, specializing in traffic.

The purpose of the traffic impact statement is to determine the proposed development's impact on traffic capacity and traffic safety as well as determine mitigating measures to improve any reductions to capacity and safety. The traffic impact statement shall include the following:

- (a) A detailed assessment of existing versus proposed traffic conditions on any significant travel ways or intersections that may be impacted due to the proposed development. The analyses will be done using accepted traffic engineering procedures as presented in the 2000 Highway Capacity Manual of the Transportation Research Board. Projected traffic for the proposed development will be calculated based upon data obtained from Trip Generation, 7th Ed. of the Institute of Transportation Engineers (ITE) or based on information obtained from a similar development. The projected trips are to be added to the existing peak hour traffic count to yield total projected traffic. The proposed conditions are to also include additional traffic generated from the proposed development and any other future development permitted or in the process of being permitted within the vicinity of the project that may impact the traffic conditions. The assessment shall also include a review of the impacts of the project on existing nearby traffic nodes.
- (b) Accident data for the roads and intersections in the vicinity of the project site will be obtained from the Cumberland Police Department for the latest three years. A

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traffic safety analyses shall be performed to ensure that no existing safety issues occur within the vicinity of the project. Analyses of the traffic accident data, including discussions with the Cumberland Police Department, will be undertaken to determine whether there are unexpected patterns for an area with the existing geometric and traffic patterns observed. The traffic safety analyses shall include review of the geometric configuration of critical locations with regard to safe stopping sight distance. Based upon principles presented in A Policy on Geometric Design of Highways and Streets of the American Association of State Highway and Transportation Officials (AASHTO) and upon observed vehicle speeds, the adequacy of safe stopping sight distance on all approaches will be determined.

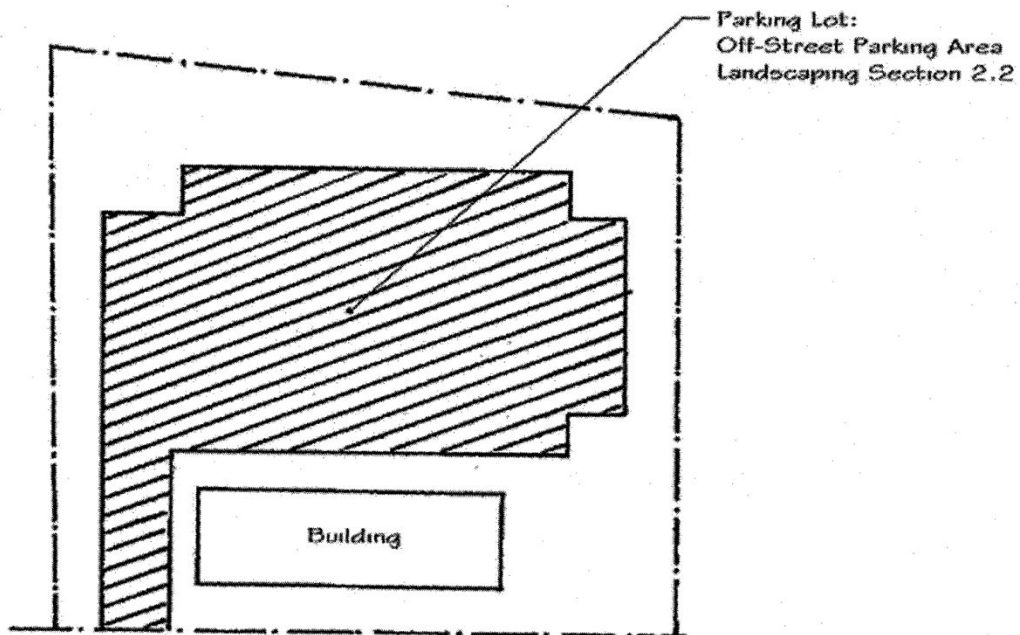
- (c) An analysis of the interior traffic circulation for a development is to be performed. The analyses is to include both vehicular and pedestrian traffic flow.
- (d) Based on the results of the abovedescribed analyses, conclusions are to be drawn that will include, as necessary, recommendations for mitigation of the impact of the projected traffic.

Roads and parking areas should be designed to respect natural features and topography, and to present an attractive “streetscape” environment. Vast expanses of paving without visual relief are undesirable. Materials should be harmonious with the existing, surrounding environment. Durable materials such as brick, granite, stone, wood, and textured/colored concrete are preferable.

- (2) Driveways: All driveways shall:
 - (a) Integrate access points for automobiles and pedestrians carefully -especially within the village centers where pedestrian and vehicle traffic co-exist. Driveways should be shared by adjacent developments wherever possible to minimize curb cuts and impervious surfaces unless safety concerns associated with incompatible land uses cannot be satisfied.
 - (b) Use special accents at all entries. Monuments, uniquely textured paving, plantings, walls, sculptures, and specimen trees should be used to generate visual interest, and conform to the surrounding context.
 - (c) Every development must have sufficient emergency access as required by existing regulations and the local fire, rescue and police departments.
 - (d) Driveway elevations at the property line shall be six inches higher than the elevation curb at the gutter line to avoid flooding of the property from street runoff.
 - (e) Separate customer access and circulation from service truck or delivery access.
 - (f) Roads and driveways should follow existing contours to minimize site disturbance and be designed parallel, rather than perpendicular, to existing slopes.

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- (3) Parking. Off-street parking shall be provided in accordance with the applicable provisions of this appendix, however, the Planning Board may recommend relief for good cause. In general, where parking areas can be reduced in size, or spaces shared with adjacent businesses, it is considered beneficial to reduce impervious surface areas and maintain a more natural appearance. In order to limit impervious surfaces, parking areas shall not be in excess of the requirements of this appendix.

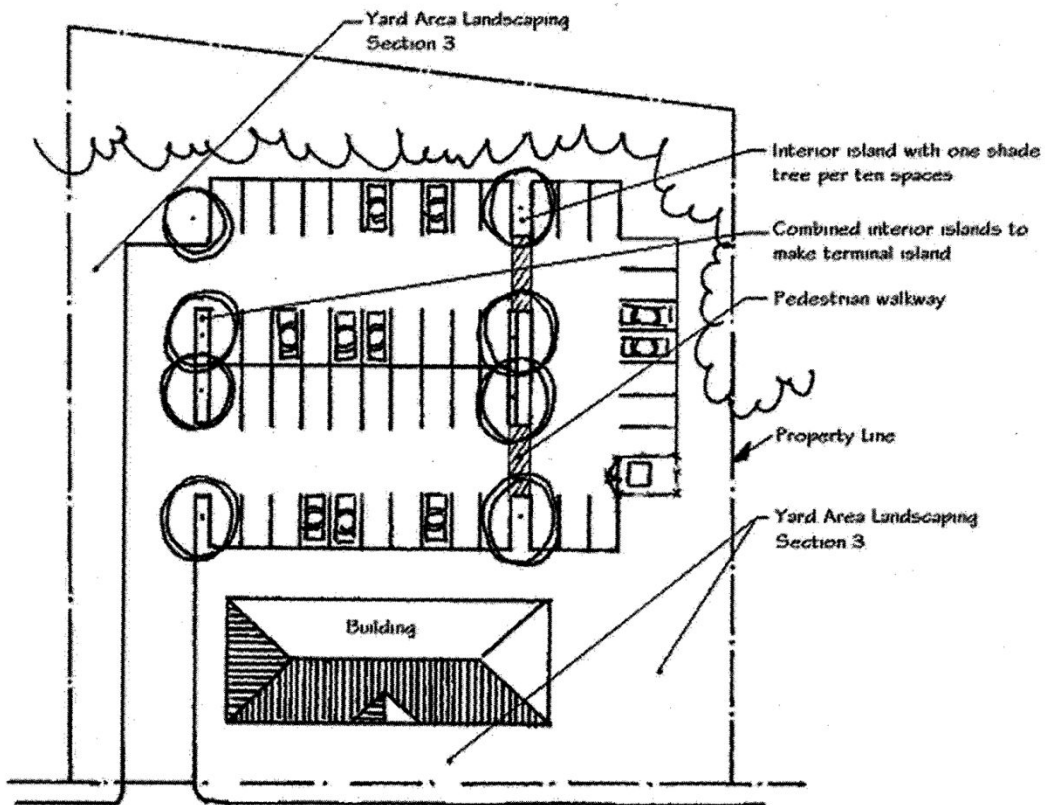


LANDSCAPING:

AREAS COVERED BY SECTION 2.2

- (a) The interior of parking areas must be landscaped. Divide large parking lots into a series of smaller connected lots using raised landscape strips at least five feet wide (preferably more) with one shade tree for every 10 spaces (or in accordance with the zoning ordinance, the vegetation may be clustered to create one landscaped area for every 20 parking spaces).

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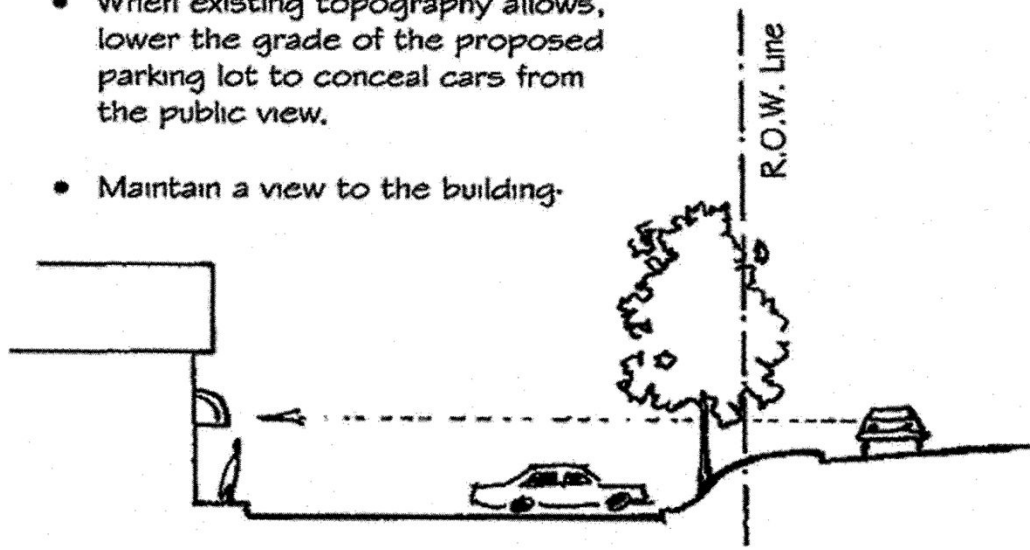


**LANDSCAPING FOR
OFF-STREET PARKING**

- (b) Each row of parking spaces shall be terminated by landscaped islands which measure not less than five feet wide and not less than 18 feet in length. Terminal islands shall be surrounded by continuous raised curbing and contain two shade trees. Interior islands and divider medians shall be protected from encroachment of motor vehicles in a manner approved by the Planning Board.
- (c) Lower the grade of parking areas or provide raised berms, where practical and respectful of existing topography, to aid in screening views of automobiles while permitting views of buildings from the right-of-way.

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- When existing topography allows, lower the grade of the proposed parking lot to conceal cars from the public view.
- Maintain a view to the building.



USE OF GRADE TO SCREEN PARKING

- (d) Parking areas must be at least five feet from buildings separated by a raised walkway, planting strip, or bollards. There shall be sufficient space directly in front of the building for emergency vehicle access. Parking areas directly abutting the building or right-of-way shall not be considered acceptable.
 - (e) Parking areas shall be located to the rear or sides of buildings out of sight from passing traffic. Vegetative buffering, berms, walls and fences should be used to screen parking to the greatest extent possible from all surrounding areas.
 - (f) Pedestrian walkways should be provided through and between parking areas, buildings and wherever possible to adjacent streets.
- (4) Drive-through facilities.
- (a) No drive-through facility shall be located in any front yard. Entrances to a drive-through facility shall be offset at least 150 feet from an intersection. No drive-through lane shall exit directly onto a street.
 - (b) A sufficient number of stacking spaces for vehicles waiting to complete a transaction shall be provided so as to prevent circulation congestion, both on-site and on adjacent public streets. In general, the number of stacking spaces shall be based on the following minimum requirements:
 - i. Restaurant: 10 spaces per station.
 - ii. Bank: five spaces for the first station, plus two spaces for each additional station.

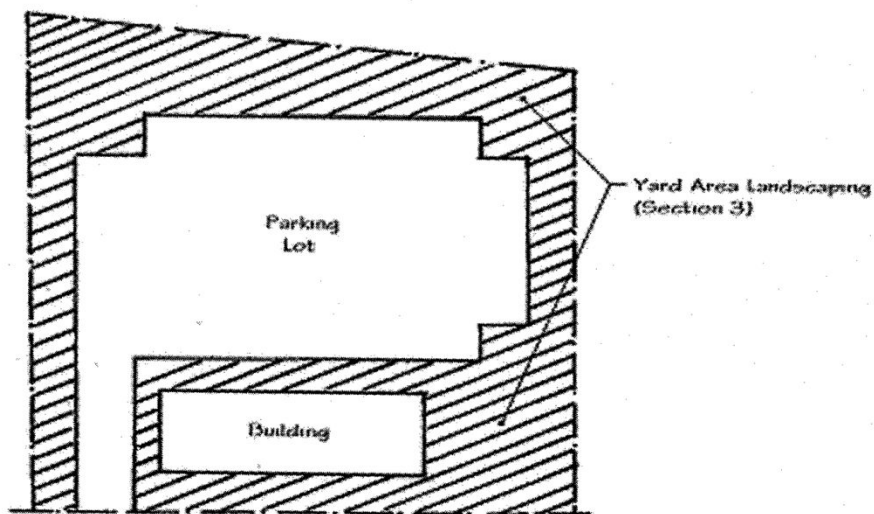
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- iii. Other uses: four spaces per station.
 - (c) In addition, there shall be at least one stacking space after the service window, before entrance to a travel lane. Each stacking space shall be a minimum of 10 feet in width and 20 feet in length.
 - (d) Drive-through lanes shall be delineated from traffic lanes and parking areas with striping, curbing, landscaping and/or the use of alternative paving material. Where pedestrians will intersect with a drive-through lane, crosswalks shall be provided, making use of striping and/or alternative paving material.
 - (e) Adequate directional and warning signs shall be provided to assure smooth traffic circulation and pedestrian safety, including marking entrances, exits and one-way lanes of drive-through areas. The placement of all directional signage shall be subject to review and approval under the provisions of these regulations.
 - (f) Menu boards or other informational boards shall face away from public rights-of-way. All lighting associated with menu boards, window service areas or travel lanes shall be directed and shielded so as to prevent any glare or reflection on adjoining streets or property. The placement of all informational signage shall also be subject to review and approval under the provisions of these regulations.
- (5) Pedestrian pathways/sidewalks. Pedestrian systems shall be clearly defined through both the natural and built environments. Attempts should be made to connect pedestrian networks between developments where there exists a logical and practical connection.
- (a) Within the village centers, sidewalks and paved pathways should be a minimum of four feet wide. Clearly defined pedestrian access should be provided to primary building entrances.
 - (b) The Planning Board may require the construction or reconstruction of sidewalks and curbing in the right-of-way as an off-site improvement.
 - (c) Informal pathways/trails should be provided to connect adjacent natural areas and potential future regional pathways and bikeway.
 - (d) Crosswalks, signs, or other warning cues should be used wherever pedestrians cross traffic aisles.
 - (e) Walkways and open areas shall be generously vegetated.
- (6) Stairways and ramps. All buildings should be handicapped accessible in accordance with state and federal laws. Provide hand railings in accordance with applicable regulations. Stairs should combine visual attractiveness with safety considerations and provide landings for visual variation and pedestrian rest.

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M. Landscaping. [Amended 9-16-2020 by Ord. No. 20-05B]

- (1) To the maximum extent possible, the natural landscape shall be preserved. Landscaping shall serve as a unifying element, creating continuous patterns along the street edge and integrating the various elements of site design into the plan with the surrounding landscape elements and processes. In built-up neighborhoods, for example, landscaping which integrates manmade materials, like brick, with plant materials, may be appropriate the closer the building is to the sidewalk. Effort shall be made to use native plants with high wildlife value and aesthetic interest. Plants shall also accent the cultural landscape, providing such elements as rhythm, spatial structure, color, texture, etc. to the built environment.

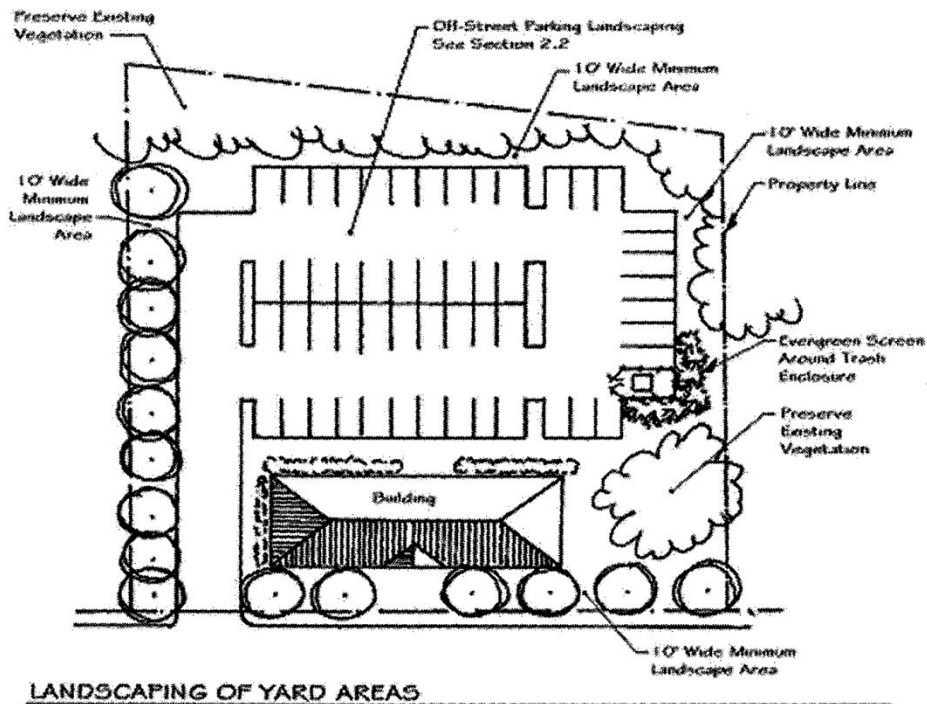


LANDSCAPING: AREAS COVERED BY SECTION 3

- (2) All areas not covered by structures, service yards, driveways, paths, or similar features shall be landscaped. The following are planting design concepts that shall be used:
 - (a) Preservation of existing exemplary or distinctive vegetation, land forms or site features.
 - (b) Specimen trees in informal groupings and rows at major focal points.
 - (c) Use of flowering vines on walls and arbors.
 - (d) Use of planting to create shadow, texture, patterns, rhythm, aroma, color, etc.
 - (e) Use of trees to create canopy and shade, especially in parking areas.
 - (f) Use of berms, planting and walls to screen outdoor areas from wind and noise.

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- (g) The development of sloped properties shall follow the natural contours of the land.
- (h) Terraced parking lots, stepped building pads, and larger setbacks shall be used to preserve the general topography of the site and to minimize grade differences between adjacent streets and properties, especially when adjacent downhill properties are residential.
- (i) Landscaping around the entire building to soften edges and moderate scale is required, particularly near parking lots, entrances, and other pedestrian areas.
- (j) Permanent planters are required for areas not conducive to permanent plantings.



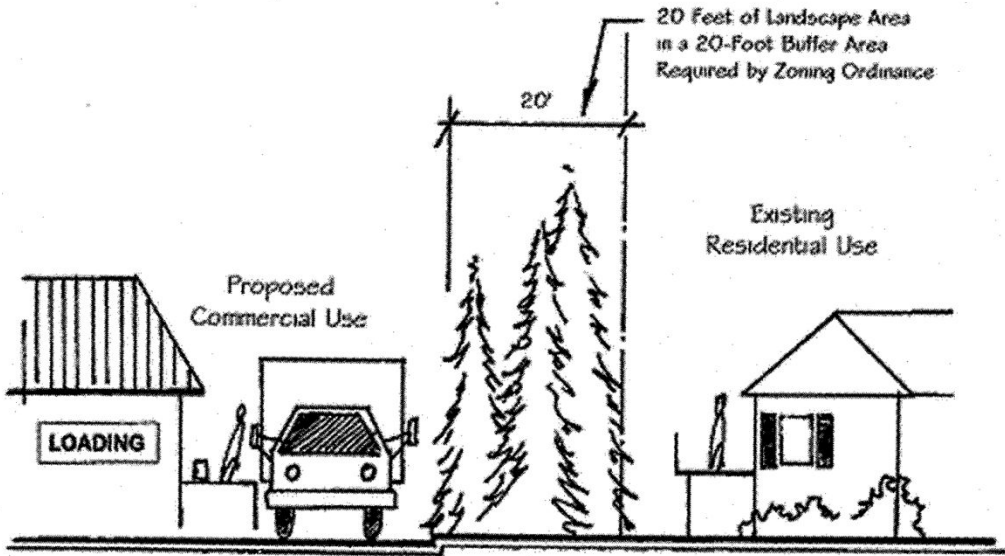
- (3) Landscaped setback yards, berms, walls, and screens. Every development or redevelopment shall provide sufficient year-round vegetated setback yards, berms, walls, and other screens to shield neighboring properties from any adverse external effects of a development, to shield the development from negative impacts of adjacent uses, to minimize stormwater impacts on flood management and water quality, when building design and siting do not provide sufficient privacy, or to aesthetically improve the site by providing barriers to undesirable land uses such as parking lots, utility areas, loading docks, trash pickup areas, and transportation corridors. These barriers may vary in materials and dimensions depending on the intensity of adjacent land uses and other design considerations. The goal shall be to provide as much landscaping as possible from undesirable land uses.

(a) [Reserved]

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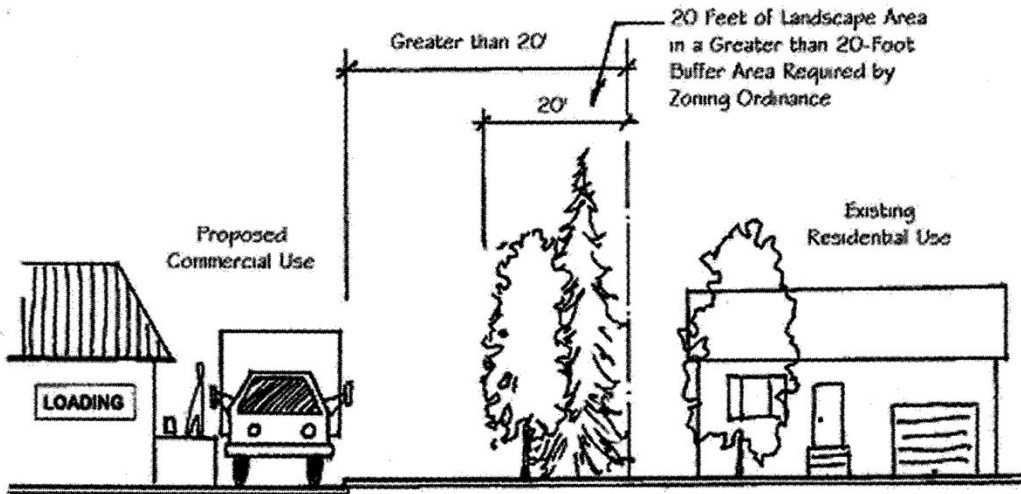
- (b) Removal of trees (over five inches in diameter) along roadways shall be minimized and in rights-of-way is prohibited.
- (c) At a minimum, landscaping of all yard areas shall be comprised of a continuous area at least 10 feet wide, except for approved driveways. On an average, this landscaped area shall be planted with grass, shrubs every 20 feet and shade trees (minimum three to 3 1/2-inch caliper) planted at least every 50 feet apart. Foundation plantings shall cover at least 60% along each side of a building.
- (d) Fences and walls shall be architecturally appropriate. Walls shall be terraced with wall sections no more than five feet in height. Chain link fencing shall be prohibited in front and side yards. No fence or wall shall be constructed or installed as to constitute a hazard to traffic or safety.
- (e) Incompatible land uses shall be segregated as much as possible and in accordance with the buffer distance requirements contained in the zoning ordinance. At least 20 feet of the buffer must be landscaped such that the incompatible uses are shielded, screened, or otherwise separated. Landscaping options for screening or buffering include, but are not limited to:
 - i. The existing land form, e.g., ledge, or existing mature vegetation.
 - ii. Evergreen trees (maximum 10 feet on center).
 - iii. Evergreen hedge, fence, berm, or wall; 36-inch maximum immediately in front of buildings.
- (f) Masonry walls consisting of stone, brick, or other similar solid masonry materials or wooden walls coupled with shrubs every 20 feet or trees every 50 feet.
- (g) Depressing the parking lot so that its elevation is approximately four feet below adjacent land use.
- (h) Riprap is not an acceptable landscaping option.

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SAMPLE CONDITION: COMMERCIAL ZONE ABUTTING A RESIDENTIAL ZONE
(WHEN THE ZONING CODE BUFFER AREA REQUIREMENT IS NOT GREATER THAN 20' WIDE)

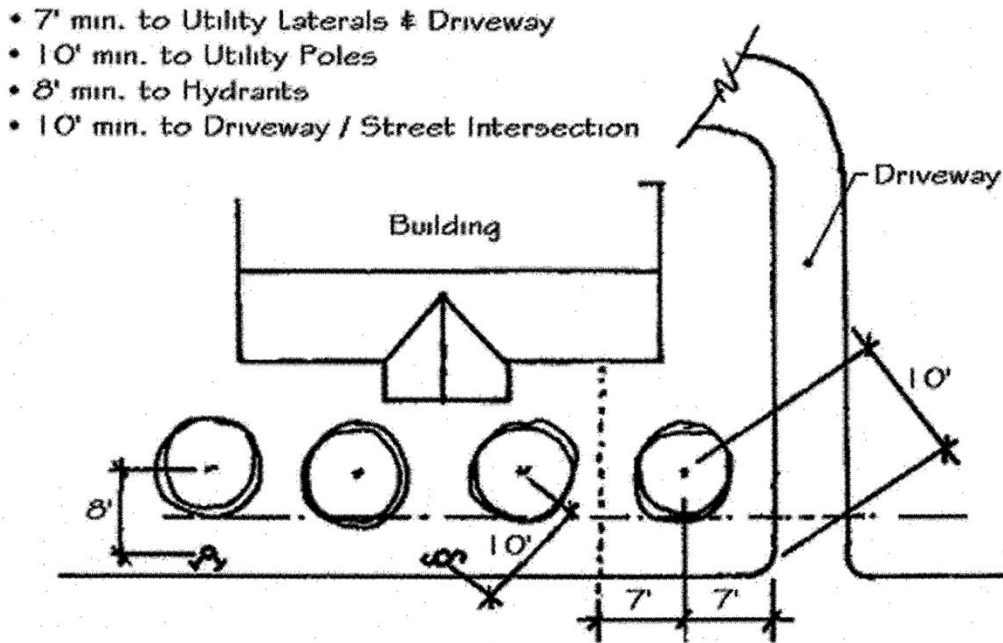
- (i) When a long expanse of fence or wall surface can be seen from a road or neighboring property, the fence or wall shall be offset and architecturally designed to prevent monotony. Landscape pockets shall be provided at fifty-foot minimum intervals along the wall. Vines or other plant material shall be used to break up flat surfaces.



SAMPLE CONDITION: COMMERCIAL ZONE ABUTTING A RESIDENTIAL ZONE
(WHEN THE ZONING CODE BUFFER AREA REQUIREMENT IS GREATER THAN 20' WIDE)

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- (4) Plant size, quality, spacing, and distribution.
- (a) Newly planted shade trees shall have a minimum installation size of 3 1/2-inch caliper, and shall be staked securely for a period of two years from date of planting. Where adjacent to pedestrian areas, the lowest branch shall be at least 80 inches above finished grade to meet ADA standards.
- (b) Trees and large shrubs shall be placed as follows:
- A minimum of 10 feet between centers of trees or large shrubs and edge of driveway, water meter or gas meter and sewer laterals.
 - A minimum of 10 feet between centers of trees or large shrubs and point of intersection of driveways and streets or walkways.
 - A minimum of 10 feet between center of trees and large shrubs to utility poles.
 - A minimum of 10 feet between center of trees or large shrubs and fire hydrants and fire department sprinkler and stand-pipe connections.



TREE & SHRUB PLACEMENT

- (c) No species of plant or large shrub shall be planted under the overhead lines or over underground utilities (as determined to be significant by Dig Safe®) if its growth might interfere with the installation or maintenance of any public utilities.

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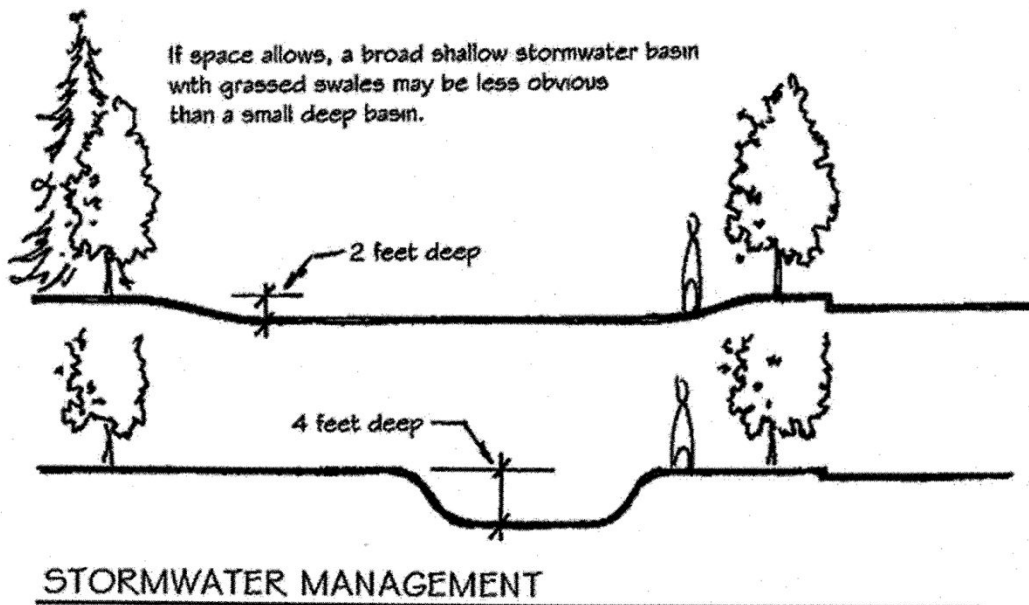
- (d) Only nursery-grown plant materials shall be acceptable, and all trees, shrubs and ground covers shall be planted according to acceptable horticultural standards. Dead and diseased plants and trees shall be removed and replaced by the owner on at least an annual basis. Failure to properly maintain landscaping may result in such work being performed by the Town at the owner's expense.
- (e) All landscaping, including that in parking lots, shall be irrigated.
- (5) Vegetation and plant lists.
 - (a) Species shall be suitable for U.S.D.A. zone 5 hardiness. Use of native vegetation is strongly encouraged, and use of observed or known invasive species is prohibited.
 - (b) Scale of plants shall be compatible with buildings and land use. Plants shall be used to moderate changes in scale.
 - (c) The approved plant list reference is entitled Sustainable Trees and Shrubs, third edition, and 1999, authorized by the University of Rhode Island Cooperative Extension Landscape Horticulture Program. A list of invasive species is available through the Cumberland Conservation Commission. Copies of the entire URI document are available through the Town of Cumberland Planning Department for a fee.

N. Stormwater management.

- (1) Natural drainage ways should be maintained in an undisturbed state to the greatest extent possible. Retention/detention basins, etc. should only be employed where the natural features cannot adequately control runoff. Stormwater management systems should:
 - (a) Be designed using non-structural or low-structural components where possible.
 - (b) Not allow downstream or off-site flooding, soil erosion or other related runoff problems.
 - (c) Improve the water quality of runoff and protect and restore the quality of ground and surface waters.
 - (d) Be designed for routine maintenance to be conducted on-site by the owners at regular intervals.
 - (e) Maintain the natural hydrodynamic characteristics of the watershed.
 - (f) Be located and designed to minimize aesthetic degradation.

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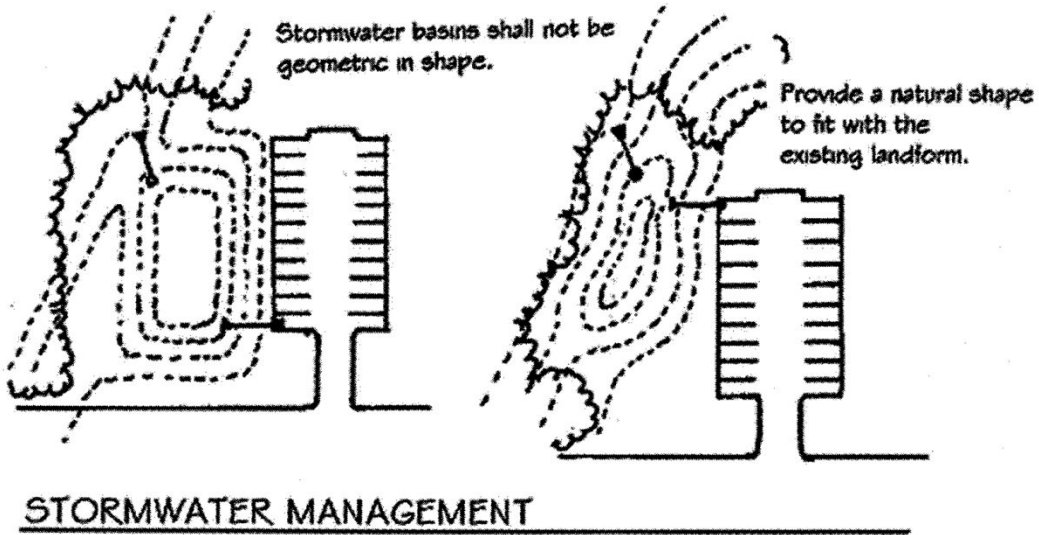
- (2) All commercial and industrial developments shall use best management practices (BMP) for stormwater management design. Refer to the “Rhode Island Stormwater Design Manual” for suggested BMPs.
- (3) Any increase in storm runoff should be retained and recharged as close to its place of origin as feasible, using one or more of the following options:
 - (a) Retention/detention basins.
 - (b) Porous pavements.
 - (c) Under-drains.
 - (d) Surface swales with infiltration drains.
 - (e) Creative pavement design which can shed surface water to vegetated areas.
 - (f) Catch basins.
 - (g) Temporary stone pads at road access point or similar techniques.
- (4) Landscaping will be required around visible, above ground control structures.



- (5) Water should be managed to decrease velocity, increase infiltration, and allow suspended solids to settle. Preferred options include grassy swales, artificial wetlands, vegetated buffer strips, extended detention basins, infiltration devices, alternative turf and wet retention/detention basins.

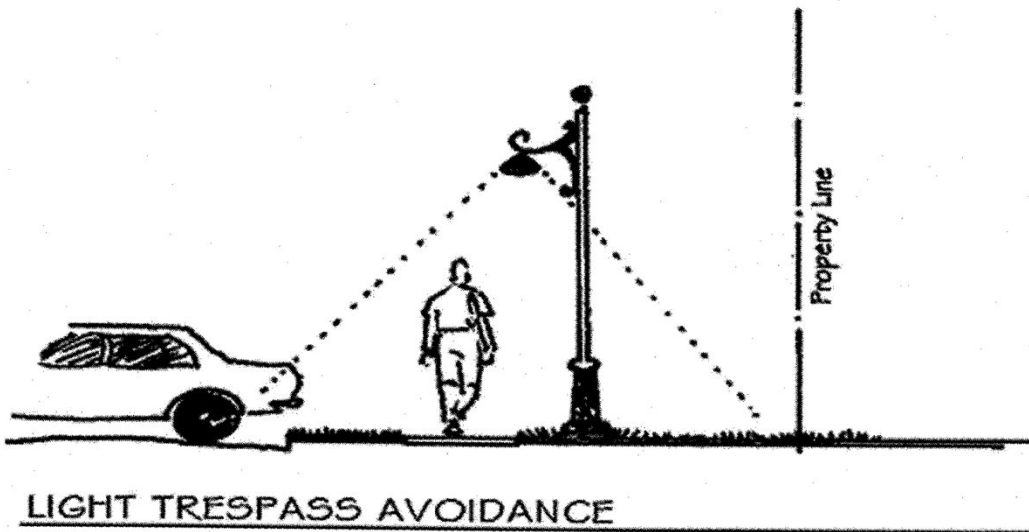
CUMBERLAND CODE

- (6) The siting and physical shape of stormwater management structures including dry and/or wet ponds and swales shall be incorporated into the natural landscape to enhance functional values of the structures and provide visual amenity to the site.



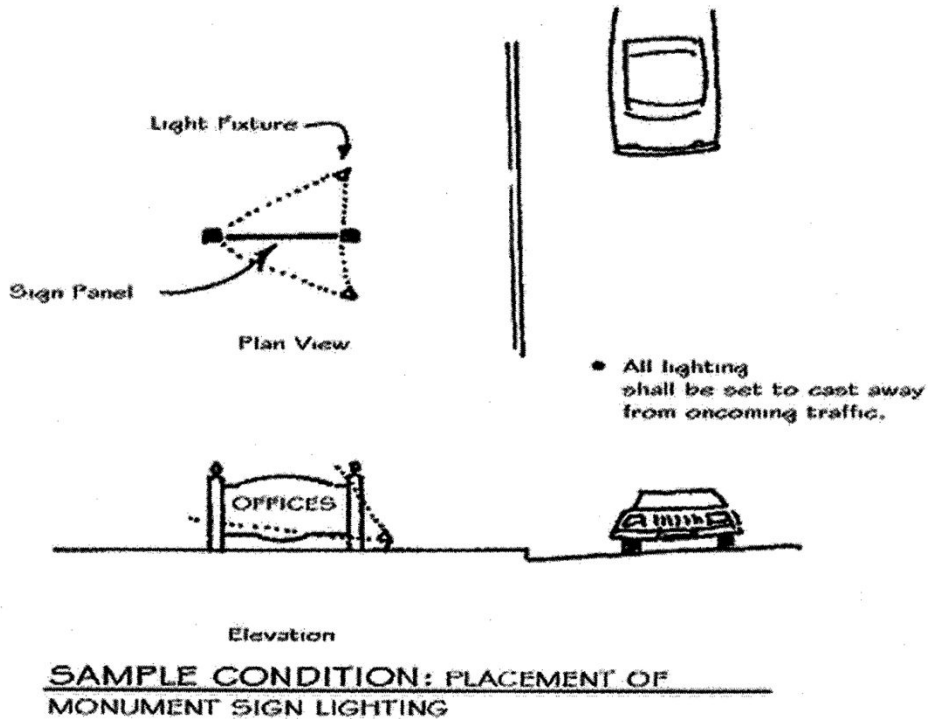
- (7) Use porous paving whenever possible. Options include porous asphalt, brick or concrete pavers set on porous base material such as sand, soil cement and gravel.
- O. Site furnishings and amenities. Site furnishings such as trellises, benches, lighting, trash containers, fencing, phone booths, etc. should be integral elements of the design and should be shown on the plans. Site furnishings shall be placed leaving adequate space for the stockpiling and removal of snow. Exterior vending machines such as soft drink and cigarette dispensers must be screened such that they do not constitute another outdoor sign or advertisement.
- (1) Lighting.
- (a) Lighting shall be designed so as not to trespass on adjacent properties or traffic. Lights should generally be directed down.

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- (b) Upward lighting, such as accent lighting shall be carefully directed away from oncoming traffic, neighboring properties and the sky.
- (c) Lamp and post height and type shall be appropriate to the architectural style of the building and be sensitive to adjacent architecture.
- (d) The lighting plan shall include the intensity and lumination expressed in foot candles and shall show all proposed lighting of the property, including the site and building. Building-mounted lighting is discouraged.

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- (2) Shade structures and shelters. Shade structures and shelters must be used for their practical and aesthetic purposes especially on large properties. Some examples include:
 - (a) Kiosks.
 - (b) Picnic areas.
 - (c) Awnings.
 - (d) Pergolas.
 - (e) Arbors.
- (3) Seating/benches. Rest areas such as benches and wall seating should be provided where pedestrians walk long distances. At least one seat for every 100 feet of pathway is a good rule of thumb. A variety of bench heights should be provided for different uses and user group.

P. Building design.

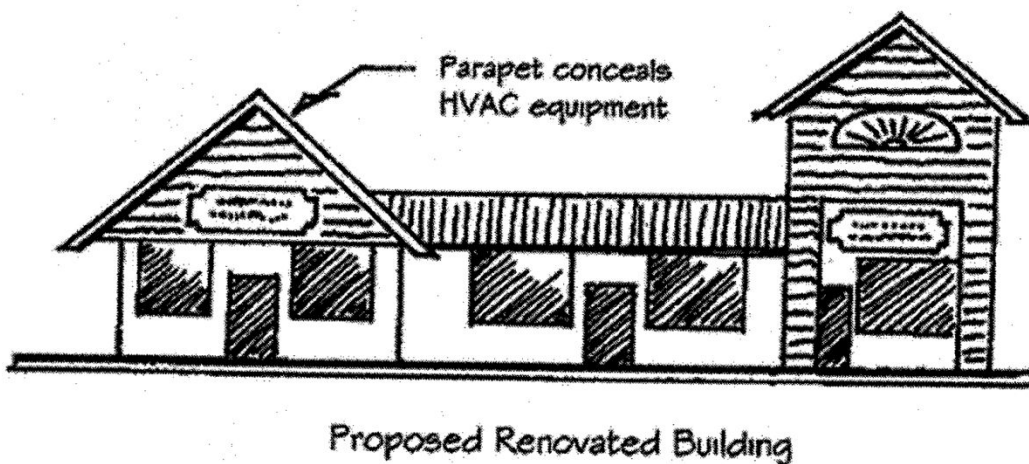
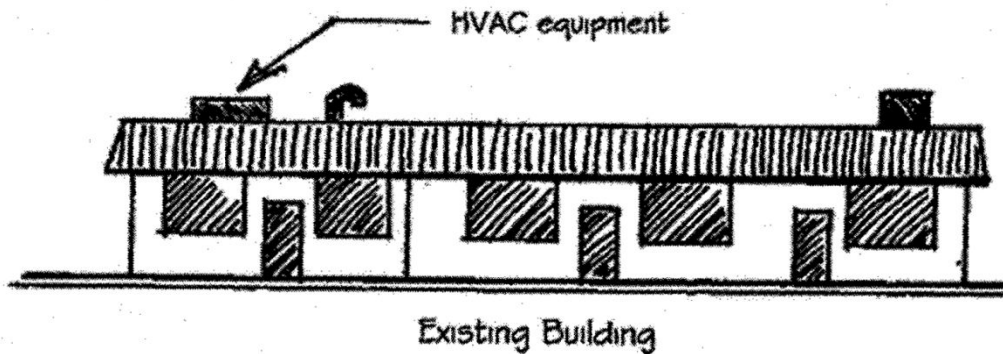
- (1) New development, modifications and expansions shall be integrated with, and complementary to, existing nearby architecture. Redevelopment projects should reuse existing buildings of character whenever possible. While demolition of buildings is governed by the R.I. Building Code, demolition of historic buildings in historic districts is also subject to historic district commission approval. Similarly, demolition

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of historic buildings outside of the historic district commission's jurisdiction is prohibited and will only be considered after a structural assessment is presented to the Planning Board by a registered architect or structural engineer. Vacant, historic buildings shall be stabilized and preserved until rehabilitated. The focus of the standards and review of building design is to maintain or improve local architectural character.

- (2) Each building or unit on a site shall have clearly defined, highly visible customer entrances featuring at minimum four of the following:
 - (a) Recesses/projections.
 - (b) Arcades, overhangs or canopies.
 - (c) Raised corniced parapets over the door.
 - (d) Peaked roof forms.
 - (e) Arches.
 - (f) Outdoor patios.
 - (g) Display windows.
 - (h) Architectural details such as tile work and moldings which are integrated into the building structure and design.
 - (i) Integral planters or a pedestrian path that incorporates landscaped areas and/or places for sitting.

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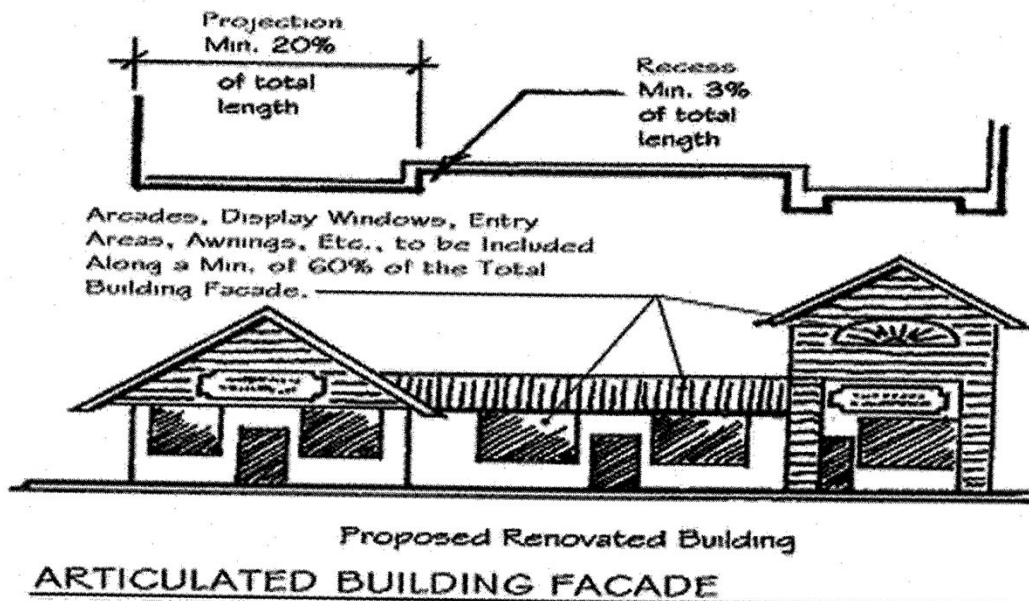
USE OF PARAPETS TO IMPROVE ROOF APPEARANCE

- (3) In general, the architectural style and materials of an existing building should be continued in an addition to the structure. Openings, such as windows and doors, are patterns on a building face and should be considered in the design of an addition. Building form, such as roof lines, should also be considered as the basis for establishing rhythm and proportion.
- (4) The scale of a new addition should be complimentary to the existing structure; the proportions should not be either too overpowering or too dwarfed. Historic elements or interesting detail should be preserved and incorporated into the overall style of the building.
- (5) Changes to a building facade should be sensitive to both the overall architectural style of the structure itself and to the architecture of the surrounding area. Historic or architecturally significant buildings shall not be covered over in a way so as to

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obscure their importance. The proportion and placement of existing doors and windows shall be maintained if those elements are replaced.

- (6) Facades and exterior walls.
- (a) Exterior elevation drawings shall identify proposed wall materials and depict proposed colors for the project. Building elevations shall indicate window locations, door locations, screening of mechanical equipment and loading dock areas. Building elevations shall be dimensioned to indicate building length and building height in addition to specifying the building roof pitch.
 - (b) Facades should be articulated to reduce massive scale and uniform, impersonal appearances of large buildings and provide visual interest that will be consistent with the community's identity, character and scale.
 - (c) Facades greater than 100 feet in length, measured horizontally, shall incorporate wall plane projections or recesses having a depth of at least 3% of the length of the facade and extending at least 20% of the length of the facade. No uninterrupted length of any facade shall exceed 100 horizontal feet.
 - (d) Commercial ground floor facades that face public streets shall have arcades, display windows, entry areas, awnings, or other such features along no less than 60% of their horizontal length. Windows, doors, and other openings should be detailed to establish them as important parts of the total composition. Design details should be employed to accentuate all entries.



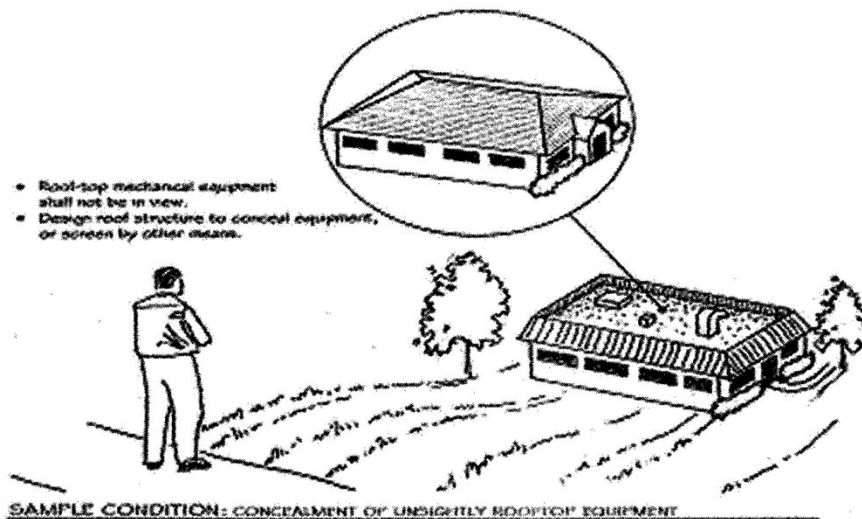
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(7) Roofs.

- (a) In general, existing roof lines should be maintained. If altered, however, the roof should be compatible in scale and form with the style of the existing portion of the building. Mansard roofs are generally discouraged on one or two story buildings. Roofs should be an integral part of the building design and overall form of the structure and should respond to the general design and nature of other roofs along the street. Roofs shall have no less than two of the following features:
- i. Parapets concealing flat roofs and rooftop equipment such as HVAC units from public view. The average height of such parapets shall not exceed 15% of the height of the supporting wall and such parapets shall not at any point exceed 1/3 of the height of the supporting wall. Such parapets shall feature three dimensional cornice treatments.
 - ii. Overhanging eaves, extending no less than three feet past the supporting walls.
 - iii. Sloping roofs that do not exceed the average height of the supporting walls, with an average slope greater than or equal to one foot of vertical rise for every three feet of horizontal run and less than or equal to one foot of vertical rise for every one foot of horizontal run.
 - iv. Three or more roof slope planes.
- (b) Where a flat roof not meant to be visible from the street is used in the building's design, decorative cornices and parapet walls should be used to delineate the building's profile.
- (c) Mechanical equipment should not be located on the roof if the building is located below grade of an adjacent road or near an adjacent building that could provide views of the proposed roof, unless the equipment can be hidden from view by building elements that are designed for that purpose as an integral part of the building's design.

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building elements that are designed for that purpose as an integral part of the building's design.



- (8) Exterior building materials and colors. Exterior building materials should be aesthetically pleasing and compatible with materials and colors used in adjoining neighborhoods. Urban environments, for example, require the use of hard edge and durable urban materials. Predominant exterior building materials shall be high quality materials, such as: brick, wood, sandstone, other native stone, or tinted, textured, concrete masonry units.
- (a) Predominant exterior building materials shall not include the following:
- (1) Tilt-up concrete panels.
 - (2) Pre-fabricated steel panels.
 - (3) Large blank walls.
 - (4) Flat roofs without a decorative cornice or parapet.
 - (5) Concrete and cinderblock walls.
 - (6) Highly reflective surfaces.
 - (7) Square “box like” buildings.
 - (8) Mixing of unrelated exterior materials.
 - (9) Exposed pipe columns.

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- (b) Building elevations should be designed to fit into the surrounding neighborhood. Architectural gimmicks, such as roof lights, distinctive roof shapes, large false cornices and parapets that sacrifice the integrity of a streetscape to promote a single structure are not allowed.
- (c) Building forms shall be designed to create and define visually attractive exterior and functional spaces.
- (d) Auxiliary structures should be architecturally consistent with primary structures on site.

Q. Signage.

- (1) Signage should be provided for both vehicles and pedestrians. Each development should work within a pre-established “sign envelope” according to the type of sign and size of the development. Envelope size should be proportional to the size of the overall development and immediate streetscape as defined in the zoning ordinance.
- (2) Signs should be simple, easy to read by passing motorists, adequately illuminated, and should complement the color, materials and design of the building architecture. Signs and their illumination shall not adversely impact public safety.



* Develop a Comprehensive Signage Program with Complimentary Colors and Design

SIGNAGE PROGRAM

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- (3) For multi-tenant buildings, a comprehensive signage program shall be developed; only one freestanding sign is allowed.
- (4) Materials shall be similar to those used in buildings. Signs shall be simple in design, although engraving, molding and other design features can provide a craftsman-like look.

R. Servicing the building.

- (1) Loading areas shall not be in front of buildings. Loading areas shall be located at the rear or sides of buildings and screen as appropriate.
- (2) Areas adjacent to residential properties should be free of service circulation unless appropriate landscaping is provided.
- (3) Dumpsters, air conditioners, HVAC equipment, trash compaction equipment and other utilities shall be incorporated into the building architecture and/or screened from view, hearing or smell with appropriate fencing, plantings or other appropriate barrier as required in § 5-2 of the zoning ordinance.
- (4) All trash and garbage bins shall be:
 - (a) Stored in an approved enclosure unless bins are stored in an approved service yard.
 - (b) Easily accessible by each tenant.
 - (c) Located away from residential areas.
 - (d) Architecturally compatible with the project.
 - (e) Screened using plant materials.
 - (f) Provided with stress pads to avoid damage to pavement.

- S. On-site storage and use of materials. No materials of a hazardous nature as defined by the Hazardous Substance Act (Rhode Island General Laws 23-24-2) shall be stored except with the explicit approval of the Town and then in strict compliance with applicable local, state, and federal regulations governing such storage.

All aboveground storage tanks containing hazardous materials should use the highest state of the art equipment to ensure safety. Facilities should include secondary containment within a vault constructed of appropriate materials, i.e., concrete.

Outside storage of materials, supplies, or equipment, including trucks or other motor vehicles, when allowed by the zoning ordinance, shall not exceed 5% of the gross floor area of the principal structure on the site. Further, equipment shall be screened on sides in harmony with the architecture, design, and appearance of neighboring structures and other surroundings.