

Town of LeRay Commercial Corridors Design Guidelines
[Added 6-12-2008 by L.L. No. 5-2008]

A. INTRODUCTION

The Commercial Corridors Design Guidelines are attached as an appendix to the Town of LeRay Zoning Code. They establish a clear vision for the town's commercially zoned areas, namely those areas of the town in the following zoning districts:

- BR-1 Business;
- BR-2 Business and Residential;
- DD Developing District;
- IND Industrial; and
- PDD Planned Development District.

The guidelines provide guidance with respect to the appearance and function of development in these areas of town that supplements the minimum requirements set forth in the zoning code. It shall be noted that the above areas encompass a variety of environments within the town and that the design guidelines shall be applied in a manner that responds to differing environments and contexts. In a number of places within the guidelines, additional guidance and direction is provided for an area of town with unique characteristics and opportunities, or for a particular type of use, such as for multifamily residential development (Section E.5 of the guidelines).

Property owners, developers, and those responsible for reviewing projects in the corridor will all benefit from a clear understanding of the expectations and vision for these important areas of town. The vision that they set forth will strengthen the town's corridors as economically productive, attractive, convenient and safe environments that the town can be proud of for years to come.

The main goals of the guidelines are to:

1. Support well planned and orderly development;
2. Encourage creativity; and
3. Ensure that individual projects and actions within the town's commercial corridors support and enhance the safety, appearance and economic viability of the corridors for residents, business owners and shoppers.

B. APPLICABILITY

The Design Guidelines assist in the design and review of both new construction and modifications to existing sites and structures along the town's commercial corridors. Specifically, the design guidelines pertain to all properties within the Commercial Corridor Design Overlay District ("CCDOD"), depicted in Figure 1 on the following page. It is the intent of the guidelines to provide substantive direction while providing reasonable flexibility, as determined by the appropriate review board, in their application.

The following activities within the CCDOD are subject to the guidelines:

1. ***New Construction***

The guidelines will be used to assist in the design and review of new construction projects, special permits, and site plan reviews in the corridor.

2. ***Alterations to Existing Sites and Structures***

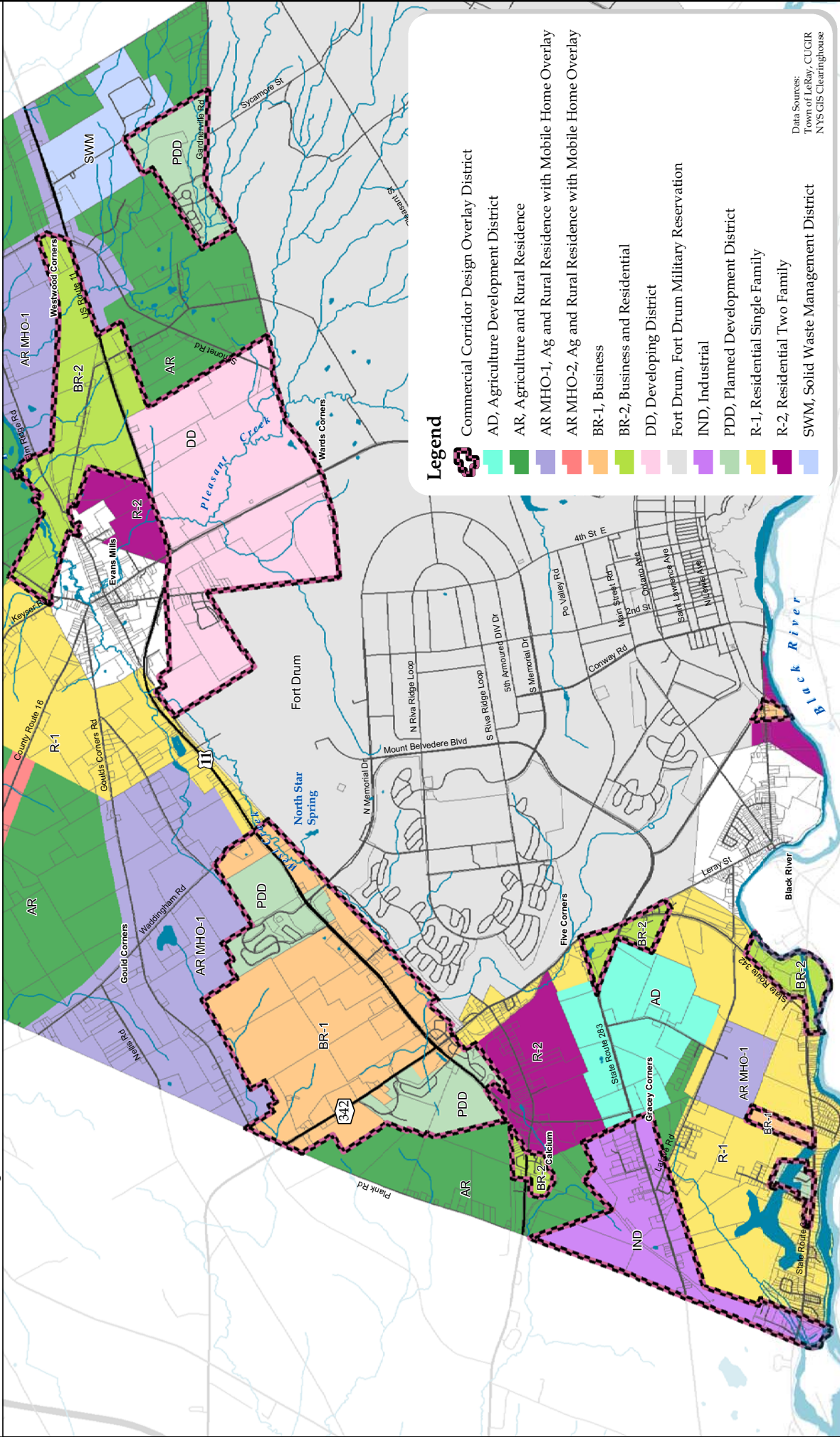
The guidelines apply to projects involving modifications to existing sites and structures that require approval from the Planning Board. Where a change of building occupancy occurs, the primary use is similar, and no significant physical change is made, the guidelines shall not be triggered. However, if a change in building occupancy occurs that requires modifications to the site or the obtaining of an approval from the Planning Board, the guidelines shall be triggered. In such cases, opportunities to bring the existing site in compliance with the Design Guidelines shall be explored, with special consideration of those items which are most feasible and would achieve the greatest overall results in the appearance and functionality of the site.

3. ***Town Zoning***

The town zoning code establishes the minimum standards and requirements for all properties within the CCDOD. The design guidelines are intended to augment the minimum requirements of the town zoning code.

The Design Guidelines establish both overarching design principles for the town's commercial corridors and guidance on the details of development that most contribute to community character and functionality. At the same time, the Design Guidelines are designed to respond to the unique conditions and opportunities of a

Town of LeRay - Commercial Corridors Design Overlay District



For Conceptual Planning Purposes Only

Map prepared by:
Behan Planning Associates, LLC
Planning Community Engineers

given site or project. It is precisely this combination of a clearly articulated overall design vision for the town's commercial corridors and room for flexibility and common sense solutions on a case-by-case basis, that gives the guidelines their strength.

Successful implementation will require collaboration and cooperation between those proposing projects along the town's commercial corridors, and the officials and staff charged with reviewing and approving them. As such, the guidelines shall be viewed as a "framework" for dialogue that sheds light on expectations and opportunities during the development review process. In the end, the guidelines are about creating long-term value along the town's commercial corridors that provides for continuing economic opportunity and innovation, while strengthening the town's quality of life and "pride of place" for years to come.

In applying the Design Guidelines, the scale and scope of the proposed project or development shall also be taken into consideration. Larger projects may warrant a more thorough application of the guidelines than smaller, more modest projects along the town's commercial corridors. The term "shall" is utilized throughout the guidelines and denotes that the guidelines are to be adhered to, to the maximum extent practicable. The burden of demonstrating why a particular guideline cannot be adhered to shall rest on the applicant.

C. ORGANIZATION

The design guidelines are organized around a set of general principles that guide the following aspects of all applicable activity within the CCDOD:

1. Traffic circulation and parking;
2. Building appearance and layout;
3. Site details such as landscaping, lighting and signage;
4. Additional provisions for corporate or franchise architecture; and
5. Multifamily development.

D. DEFINITIONS

The following definitions clarify a number of the terms utilized in the Design Guidelines. Applicants proposing development in the town's commercial corridors shall also consult the definitions sections provided in conjunction with the town's Subdivision, Zoning and other applicable regulations.

- *Arcade* – an area contiguous to a street or plaza that is open and unobstructed, and that is accessible to the public at all times. Arcades may include building columns, landscaping, statuary and fountains. Arcades do not include off-street loading/unloading areas, driveways or parking areas.
- *Buffer* – see also "screen". An area provided to reduce the conflict between two different land uses. Buffers are intended to mitigate undesired views, noise and glare - effectively providing greater privacy to neighboring land uses. Typical buffers consist of materials that serve this purpose and include, but are not limited to, plant materials, walls, fences and/or significant land area to separate the uses.
- *Buffer Strip* – a portion of a lot or property used to visually separate one use from another through the use of vegetation, distance or other approved method.
- *Dormer* – a window set vertically in a gable projecting from a sloping roof.
- *Franchise Architecture* – architecture employing a distinct architectural building style, colors and/or other elements commonly employed by a fast food or other retail franchise that serve to promote brand identity.
- *Gable* – a triangular wall section at the end of a pitched roof, bounded by the two roof slopes.
- *Hip Roof* – a roof constructed of four sloped sides, coming to a point or a ridge.
- *Mansard Roof* – a roof with two slopes on all four sides; the lower slope generally curved and much steeper than the upper slope, which appears flat on top.
- *Parapet* – the portion of a wall that extends above the roofline.
- *Public/Private Right of Way* – any public or private road, access easement intended to provide public access to any lot/development, but excluding any service road or internal driving aisles (i.e., within parking lots).
- *Screen* – see also "buffer". The sole purpose of a screen is to block views. A screen shall be constructed of opaque materials and whose height will be effective in obstructing unwanted views.

E. DESIGN GUIDELINES

1. Circulation and Parking

Intent: The town's commercial corridors do double-duty – they serve as both a venue for commercial activity and investment, residential environments, and as important transportation corridors in the town. Route 11, in particular, serves as a major link between important locations in the town, including Ft. Drum, Interstate Route 81, and population centers such as Watertown and the Village of Evans Mills. The town recognizes the need to provide a transportation network that facilitates safe and effective traffic movement along its commercial corridors, while at the same time, creating an attractive and user-friendly environment that makes shopping, doing business and living in them an appealing experience for all, including drivers, pedestrians and bicyclists.

a. *Provide Interconnected Service Drives*

Route 11 is a major State Highway and is thereby constrained in its ability to provide multiple curbcuts for individual businesses. Other town roads, while presently less developed, would also be better served by fewer curb-cuts. Therefore, in the interests of public safety, health and welfare, parallel service roads and interconnected parking shall be provided as determined by the Planning Board in consultation with the Town Board.

The locations of such parallel service drives shall be determined on a case-by-case basis. With respect to Routes 11 and 342, however, the general locations and concepts for the parallel access roads are provided in the US Route 11 & NY Route 342 Corridor Study, which is on file at the town.

Moreover, it is along these service roads that pedestrian scaled amenities such as sidewalks, bicycle paths, lighting and public gathering spaces shall be provided. Such amenities will ensure that this parallel service drive is more than just a conduit for automobiles, but an appealing and inviting environment. In the event that proposed pedestrian amenities are not initially provided, an adequate width of landscaped area shall be reserved along the service drive so that such amenities can be provided at a future date.

b. *Clearly Define Vehicular, Pedestrian, and Bicycle Patterns*

Large, unbroken expanses of parking lots are a common feature of today's commercial landscape. All too often, such areas are ill-defined, leading to confusion for both drivers and pedestrians. Bringing structure to such environments by providing clear, well-defined pathways for all users is key.



This shopping plaza has provided clear pedestrian pathways with landscaping that breaks up a large expanse of parking.

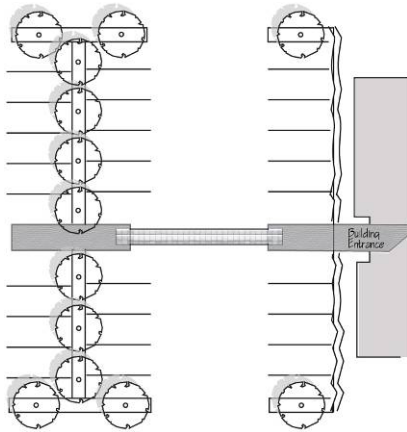
Specifically, the following guidelines shall be followed to create clearly defined patterns for both vehicles and pedestrians:

- The corridor and all major internal drives shall be lined with trees, sidewalks, lighting, and landscaped areas with trees, shrubs, flowers and ground cover.
- Internal drives and walkways off of the main highway shall form an interconnected network to distribute automobile and pedestrian traffic. These drives shall be laid out in a manner that is safe, convenient and that fosters a sense of place and acts as an extension of the public street network. Pavement markings, curbing and traffic signs shall be utilized to prevent conflicts.



Elements of a pedestrian oriented streetscape include curb bump-outs, crosswalks, planted medians and sidewalks.

- Pedestrian safety shall be promoted through the use of traffic calming devices such as bump-outs, crosswalks, speed-bumps and pavement changes, particularly to discourage high traffic speeds on internal access drives.
- Pedestrian crossings and walkways shall be distinguished from driving surfaces through the use of durable, low maintenance, surface materials such as pavers, bricks, or scored concrete to enhance pedestrian safety and comfort, as well as the attractiveness of the walkway. Signs shall be installed to designate pedestrian walkways, where appropriate.
- Pedestrian walkways shall be provided to the principal customer entrance of a building from the parking lot, and to connect adjacent lots and structures. Landscaped areas with trees, shrubs, benches, flowerbeds, ground cover, or other such materials shall be provided to buffer walkways located in parking lots.



In the example above, a clear pedestrian path links the parking lot to the building it serves. In the example to the right, pavement textures and landscaping define the pedestrian path that connects adjoining commercial buildings.





Benches, ample sidewalks, covered walkways and landscaped courtyards enrich the pedestrian environment at the building edge and along the pedestrian pathway in these examples. Parking lots, service drives and the interface between buildings and parking all benefit from the addition of such elements.



c. *Provide Connectivity*

In order for the town's commercial corridor's to function efficiently and conveniently, vehicular, bicycle and pedestrian connections between individual commercial uses shall be provided. Connectivity between commercial uses and residential areas is also important, particularly multifamily.



A continuous pedestrian/bicycle activity path is appropriate in areas with large setbacks. A more intimate interface between structures and the pedestrian realm can occur along internal service drives.

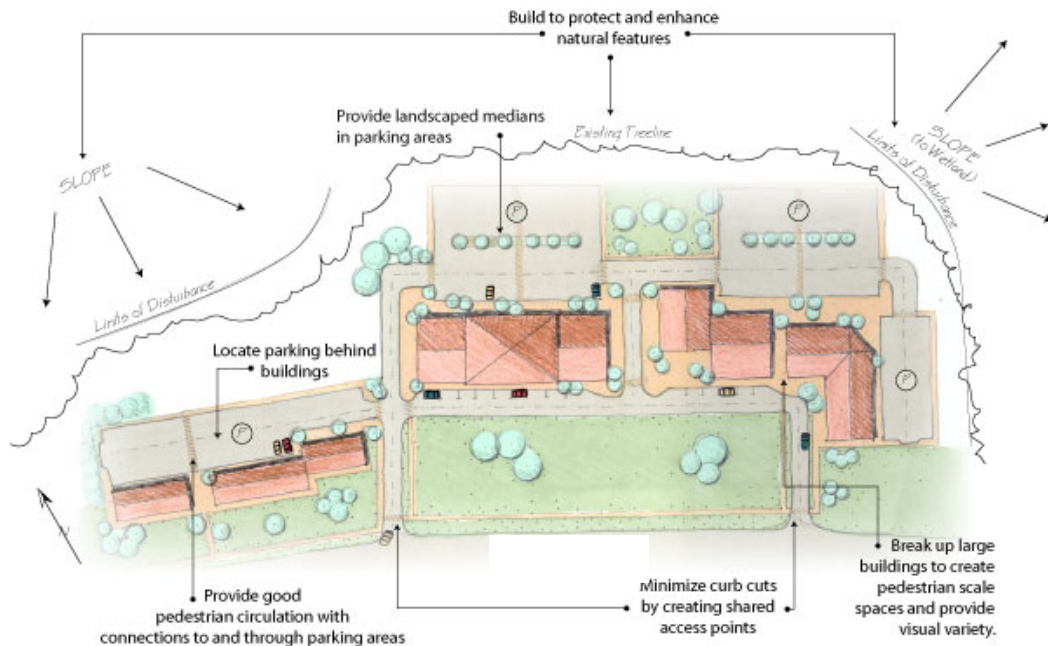
The following guidelines shall be followed to ensure connectivity:

- Parking lots of adjoining uses shall be connected to allow for convenient circulation between commercial uses and parking lots, and to maintain safety and efficiency on the town's roads by reducing traffic and multiple curb cuts. The town may require proof of shared parking agreements or easements.
- Parking lot connections shall be provided in a manner that reinforces a clearly defined pedestrian and vehicular circulation pattern across multiple properties.
- Vehicular and/or pedestrian links shall be made with adjoining residential areas and transportation networks.

- Access to public transportation (e.g. bus service) shall be provided at visible, attractive and safe locations within the corridor.

d. *Mitigate the Visual Impact of Parking*

- Parking shall be secondary to the buildings and pedestrian systems. In general, no more than 60% of off-street parking area for the entire property shall be located between the front façade of the principal building and the primary abutting streets, unless the parking lot is screened from view by out-lot development (such as restaurants) and additional tree planting and/or structures.



Parking shall be located behind buildings. A connected drive behind buildings (in addition to the main access drive located in front of buildings) provides an alternate route, particularly for service vehicles.

- Parking areas shall provide safe, convenient and efficient access for vehicles and pedestrians.

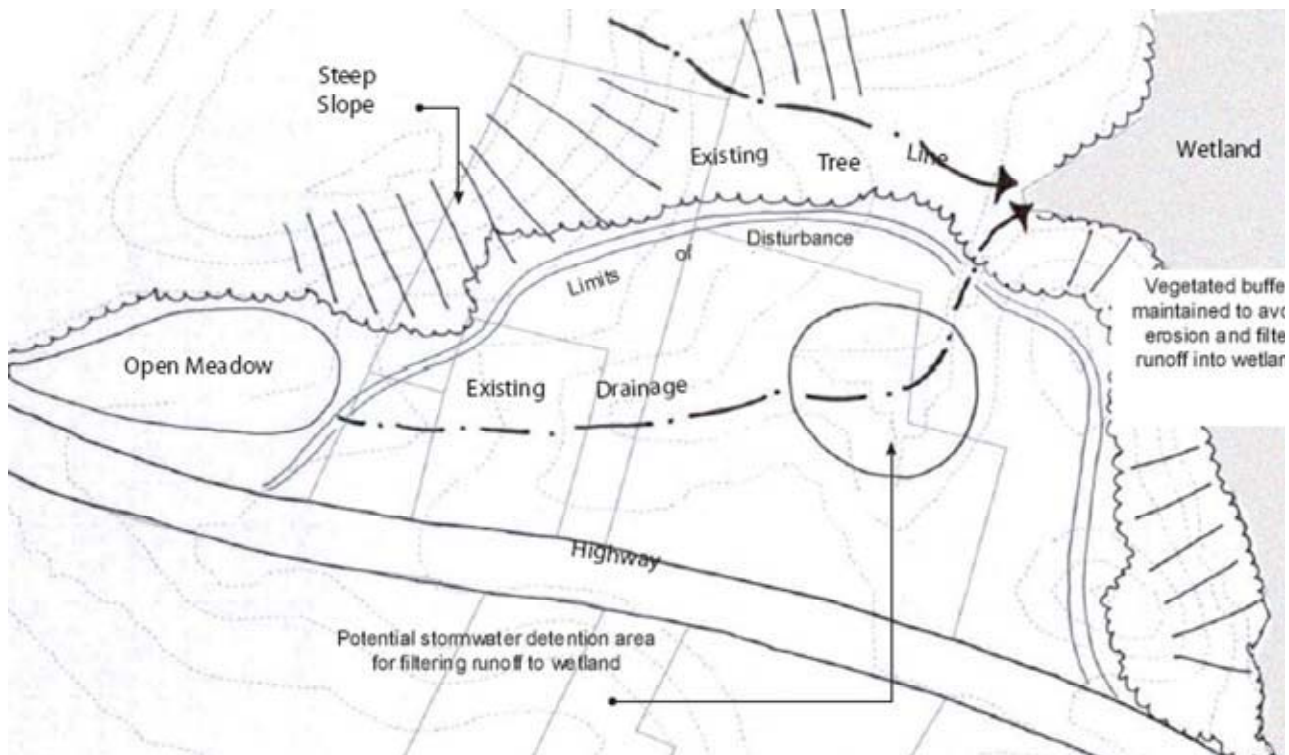
- Landscaped islands and other pervious surfaces on the site shall be used as opportunities to treat stormwater in an environmentally friendly matter and assist in water table recharge. Where feasible, pervious surfaces shall be used instead of impervious surfaces.
- e. *Encourage and Provide for Pedestrians, Bicyclists and Other Alternative Transportation Users*
- The town's policy is to provide a continuous pedestrian path along the east side of Route 11. The pedestrian path would connect businesses fronting Route 11, as well as tie into interior pedestrian circulation systems. Applicants in this area shall reserve a 5-foot strip for a sidewalk and/or path that would be built by the town at a future date. NYSDOT approval shall be required and obtained in areas where the proposed path is within the DOT right-of-way.
 - With respect to sites that are developed along internal service drives and roads, the town's policy is for applicants to incorporate pedestrian paths and/or bicycle paths as part of the overall development plan for the site. Because of the variation in conditions from site-to-site, as well as the incremental nature of development, the exact locations and dimensions of such amenities will be determined in the early stages of the development review process.
 - Bicycle racks shall be provided in easily observable locations to serve large commercial, office and multifamily areas and sites, and near school and office entrances.
 - Sidewalks shall never be designated as a multi-use path for bicyclists.
 - Grassed areas with a hitching post for horse-drawn vehicles shall be provided at key locations.

2. Site Development, Site Layout and Building Appearance

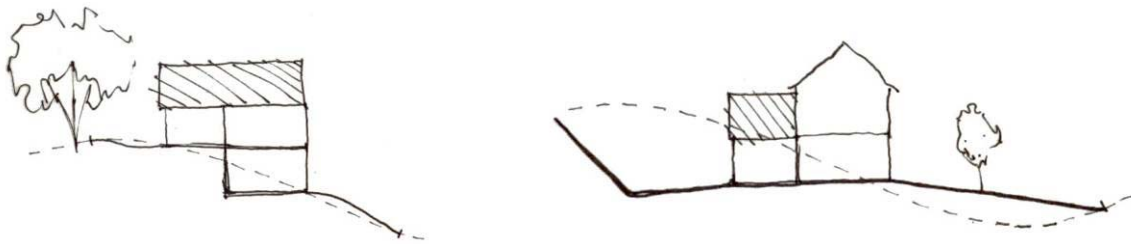
Intent: Site layout shall respond to existing natural conditions, preserving such conditions and incorporating them into the site plan to the maximum extent practicable. Sites shall also be laid out and buildings shall be designed in a manner that responds to the surrounding built environment and furthers the goals of an attractive, pedestrian friendly environment. High quality design and layout on individual project sites will collectively yield a higher quality and long term value for the town's commercial corridors.

a. *Environmental Sensitivity*

- Site design shall recognize and respect the site's natural features such as wetlands, topography, existing trees, etc., and utilize, to the maximum extent practicable, natural features and methods for managing stormwater and minimizing erosion and impervious surfaces.



The above site analysis illustrates how existing features and drainage patterns of the site can help determine limits of disturbance. By respecting the natural drainage patterns water quality may be protected with minimal cost. The potential stormwater detention area noted in the illustration would work with existing drainage patterns, providing a filtering mechanism before runoff flows into the wetland.

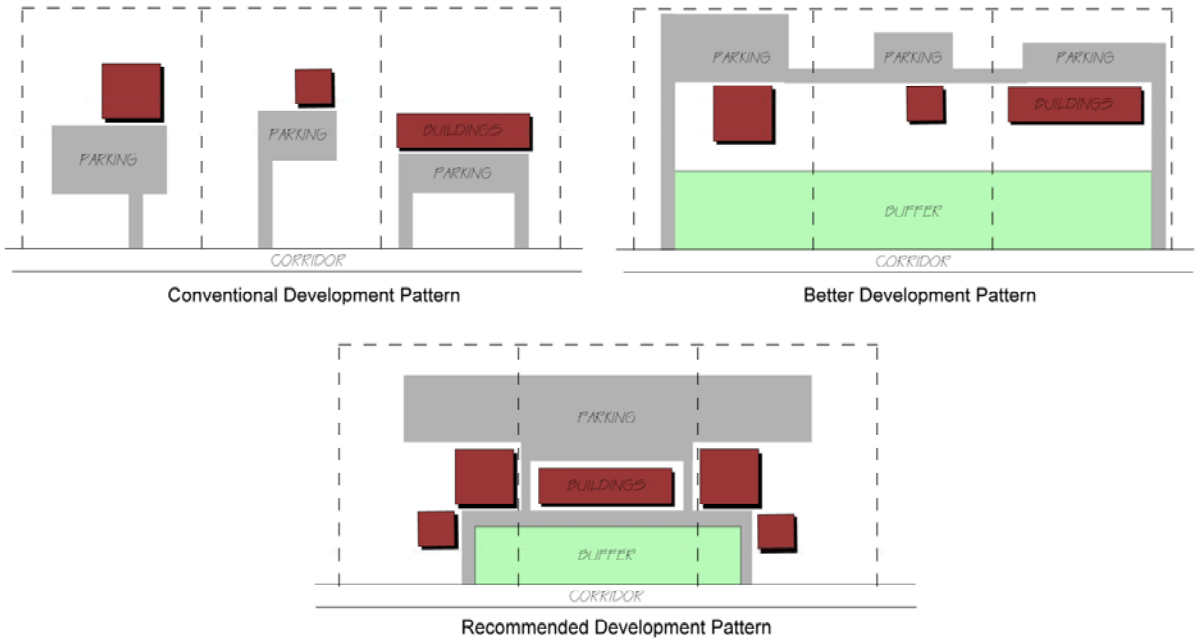


The example on the left shows how an alternative development plan for a commercial site can preserve existing topography rather than re-grading an entire site and clearing existing mature trees.

- Sedimentation and erosion control BMP's (best management practices) shall be utilized to the maximum extent possible
- Existing forested areas shall be preserved to the maximum extent practicable, particularly contiguous areas of mature growth.

b. Site Layout

- Buildings shall be related to each other in consideration of pedestrian and vehicular circulation. Frequently the layout of buildings is designed with the automobile in mind while disregarding the pedestrian realm. Supply a mix of uses within buildings close to each other to encourage pedestrian movement between businesses.
- The primary building entry shall face a street or traveled way, with secondary entries facing side and rear oriented parking lots, as illustrated on the following page.
- Transition zones consisting of sidewalks and building entry areas shall be incorporated into the site design of all development. Such provisions shall allow for pedestrian corridors and other design elements such as plantings, benches, trash receptacles, bike racks, signage, street trees and other design elements ("good" and "bad" examples on the following page).



Buildings, parking lot(s) and open space shall relate to one another in such a way as to create the feeling of one unified development and minimize entry points from the main corridor.



No Transition Zone



Good Transition Zone

A transition zone consisting of pedestrian amenities and landscaping shall occur between buildings and parking areas.

c. *Building Materials and Colors*

- Accents in the plane of the facade, created through the use of textured materials (such as the stone vernacular of LeRay) or decoration (such as brickwork patterns), are encouraged.
- Building colors shall complement colors used on adjacent structures and shall be of a natural, muted shade, such as low reflectance, subtle, neutral or earth tones.
- Building trim and accent areas may feature brighter colors, including primary colors. However, neon tubing shall not be an acceptable feature for building trim or accent areas and the use of light intensity colors, metallic colors, black or fluorescent colors is prohibited.
- Harsh, glaring white or primary colors on large surfaces shall be avoided.



Stone structures are a defining characteristic of the local vernacular architecture and are an appropriate template for design and materials of new buildings.



The wood and stone building facades in the example to the left contribute to the high quality of this commercial setting. Additional accents and building details such as trim, window mullions and entrance overhangs add to the visual interest and help to establish a "sense of place."

- Accents in the plane of the facade, created through the use of textured materials (such as the local stone vernacular) or decoration (such as brickwork patterns), are encouraged.
- Predominant exterior building materials shall be high quality materials. These include without limitation brick, wood, sandstone, other natural stone, tinted and textured concrete, or masonry units.



In these examples, higher-quality, varied building materials provide visual interest and relief to larger retail buildings.



d. *Rooflines and Massing*

- Peaked roofs are generally preferred to flat roofs, particularly with respect to larger structures. Creative use of parapets, mansard roofs, gable roofs, hip roofs, or dormers and other roof line elements shall be used to highlight entrances, add visual interest, hide rooftop mechanics from view, and break up the mass of large buildings.
- Multiple entry points shall be provided to break up the mass and scale of large retail structures.
- Antennas, satellite dishes, air handling units, exhausts and other mechanical equipment placed on the roof shall not be visible from the street, parking areas or the internal roadways and sidewalks.
- The roof of a structure shall be designed so as to divert the fall of rain and snow away from pedestrian areas such as walkways and front doors.



In this example, multiple building planes, varied building materials, and ample windows create visual interest and de-emphasize building mass, while actively engaging the public realm.



This fastfood drive-thru, with its gabled roof, varied building materials, landscaping, ample windows and an outdoor eating area, makes a positive contribution to its surroundings.

e. *Windows*

- Windows add visual interest, engage the public and break up the monotony of large building facades.
- The ground floor facade and in particular the entry, shall have the highest amount of window area and depth, shall be open and inviting, and shall have large display windows to glance into.
- Windows shall be recessed and shall include prominent sills, shutters, or other such forms of framing.
- Displays shall be used to contribute to the visual interest of exterior facades.



Windows shall be used to provide interest and visual relief. Large, unbroken expanses of window-less building façade shall be avoided.



This “big box” retail building provides a lively street level atmosphere with large windows, an ample sidewalk, landscaping and decorative awnings.



f. Loading Areas and Outdoor Storage

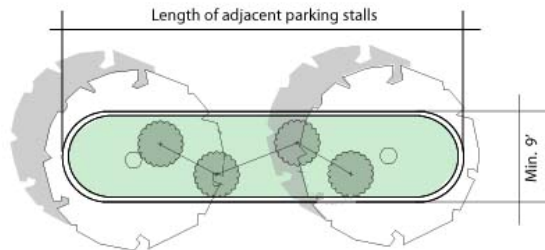
- Loading areas and outdoor storage areas exert visual and noise impacts on surrounding neighborhoods. These areas, when visible from adjoining properties and/or public streets, shall be screened, recessed, or enclosed.
- While screens and recesses can effectively mitigate these impacts, the selection of inappropriate screening materials can draw more attention to the very item they attempt to screen. Therefore, materials, color, design of screening materials, and enclosures shall conform to those used as predominant materials and colors of the building. Landscaping can also be combined with screening materials to provide additional screening.
- Areas for outdoor storage, truck parking, trash collections, loading or other such uses shall be screened from public view and public or private rights-of way. Placement of such facilities is generally most appropriate in the rear yard of structures, or the side yard, if the distance between adjacent buildings is less than 40 ft.
- All outdoor storage areas and facilities shall be located a minimum distance of 20 ft from a public internal access road, street, sidewalk or internal pedestrian path.

3. Landscaping, Lighting and Signage

Intent: *Landscaping is an integral part of any site plan. Trees and shrubs soften parking lots, define pedestrian and automobile circulation, and are environmentally beneficial. In commercial corridors, consistent, high-quality landscaping can act as a character defining, unifying element.*

a. *Landscaping*

- Existing, healthy, mature trees shall be preserved and incorporated into site development plans where feasible. Trees shall be provided with a 6' mulch area, and where possible, be located at least 6' from paved surfaces.
- In areas of high pedestrian traffic, protective measures such as curbing to define tree planters, tree grates to allow infiltration of water and protection of roots, trunk protectors and guide-wire shall be used.
- Parking lots containing 25 or more parking spaces shall provide 1 shade tree (oaks, maples, ashes, elms, etc.) for every 10 parking spaces. This is in addition to ground cover, shrubs and hedges which are to be provided where appropriate to serve their intended function while not interfering with safe sight distance for pedestrians and vehicles. Flexibility in design and location of landscaping and islands may be needed depending on the site. However, the overriding goal of providing adequate landscaping both around and within parking areas shall be adhered to.
- Opposing rows of parking in every fourth row of parking shall be separated by a landscaped median (typical width 6 feet minimum). As shown in the example on the following page, raised landscaped islands shall be located at the ends of each parking bay which contains 10 or more spaces to help define the traveled way. In addition to shade trees and grass, landscaped islands may be planted with flowering trees and/or other plantings, as appropriate. However, such landscaping shall not be used to satisfy shade tree requirements.



Landscaped islands – here with optimal dimensions – shall be provided at the end of parking bays of 10 or more cars.



- Retention basins shall be attractively landscaped (see example below) and, where feasible, double as environmentally beneficial habitats. In cases where a retention basin must be fenced for safety purposes, chainlink fencing is discouraged.



Alternative to Chain Link

The detention pond on the left, minimally landscaped and surrounded by a chainlink fence, could be more attractive with a decorative fence and additional landscaping, as shown on the right.

- Organic mulching materials are preferred over stones or other non-organic materials. The exception to this is in areas where smoking is explicitly permitted. In such areas, non-flammable mulching materials shall be utilized.

- Buffer plantings, appropriate fencing/walls, or a combination of the two shall be employed to visually screen parking lots by changes in grade, berms, wall/fences and or plantings.



- Decorative annuals shall be used for pops of color and visual interest in locations with high visual impact, such as site entrances, building foundations and at the bases of signs.



Substantial landscaping along the commercial corridor continues in the interior parking and shopping area, making an otherwise ordinary shopping plaza an inviting and attractive environment.

b. *Lighting*

- Light fixtures shall include cutoff shields that prevent glare. Light sources in canopies shall be fully recessed.
- More numerous, shorter and lower powered lighting fixtures are preferred over fewer, tall, high powered fixtures. However, in larger scaled parking lots, taller lighting fixtures may be necessary to comply with the town's minimum lighting levels as set forth in the town's zoning.
- In general, light fixtures shall not be higher than the buildings they illuminate.
- Light fixtures provided in conjunction with pedestrian paths and plazas shall be scaled for a pedestrian environment.
- Harsh white lighting fixtures that cause glare are discouraged.

c. *Walls and Fences*

- Consider low walls to suggest spatial separation or definition of territory such as seating walls or planting walls.
- Allow for surveillance by police and emergency access when planning security fences and gates. Use wrought iron or cast iron fences and gates in prominent locations instead of chain link.
- Provide a 6'- 8' masonry wall or landscape buffer for noise attenuation and visual screening where commercial loading or parking areas abut residential uses.



Landscaping and a low stone wall define the pedestrian pathway and buffer the parking lot behind it.

- Reduce the perceived scale of high walls with planters, benches or berms.
- Provide a landscaped area in front of walls abutting a public street.
- Avoid long monotonous walls. Vary the height, elements and alignment of the wall; include gates or other penetrations; repeat modular patterns along the length of the wall.
- Design walls to complement adjoining architecture in materials, detail and proportion.
- Modify chain link fencing where its appearance is objectionable. Color coat fence dark green or black where transparency is desired for view of open space. Where screening is desired, vines can transform the fence into a green wall.

d. *Signs*

- Lower, monument style signs are preferred where visibility from the right-of-way is not an issue. Preferred monument signs are low, horizontal with raised lettering, and set off by flowers, shrubs, or lawn.
- Sign materials shall relate to the materials and style of the building(s) they serve.
- The pole element of a pole-mounted sign shall be architecturally pleasing and in proportion to the sign that it carries.
- Ample landscaping shall be provided at the bases of signs.
- In general, multiple tenant signs are discouraged, especially where building mounted signs are easily read from the main traveled way upon which the



While internally illuminated signs are acceptable, building mounted signs can appear more attractive if illuminated externally.

businesses front. In such cases, a single, plaza sign, denoting the name of the plaza, is preferred.

- Keep signs – simple. Too many combinations of colors, type faces and symbols can result in visual chaos. Signs are more legible if upper and lower cases letters are combined and the image area does not exceed ½ of the total sign area.

The examples below, and on the following page, illustrate “good” and “bad” sign designs and practices:



Signs shall be adequately landscaped at their bases. The larger the sign, the more landscaping is needed.



This low, monument style sign incorporates elements of the adjacent stone wall.



Too busy

In cases where there are simply too many individual retailers to fit on one sign, or where a more unifying look is desired, "plaza signs" are suitable and often preferable to multi-tenant signs.



Bad



Good

4. Corporate and Franchise Commercial Development

Intent: The town wishes to foster high-quality commercial areas that complement and build upon the town's unique character. The architecture and site design of corporate and franchise businesses are no exception to this rule. Adherence to the guidelines set forth in this document will ensure that all commercial development, including franchises, will contribute positively to the town's character. However, the provisions below provide additional guidance for franchises, or any other business that seek to draw attention through the excessive use of colors, logos or other "branding" techniques:

a. *General Provisions*

- Corporate/business logos and colors shall not envelope the entire commercial structure, but rather, shall be limited to building mounted and free standing signs. Restrained and subtle use of corporate colors may be used on commercial structures or other elements of the site, provided that they are not excessively bright or overly applied and comply with Section 2.c of the guidelines above.
- Alternatives to the standard prototypes for building design and site layout shall be considered in the design and approval process.
- Examples of "good" and "bad" franchise architecture and site design are provided below and on the following page:



(Left) This commercial structure, home to several national chains, complements the rural landscape through building design, setbacks and the incorporation of rural elements such as a stone wall and mature trees.

(Right) This example demonstrates a national chain's successful interpretation of the local vernacular architecture and character.



Yes



No

The example on the left illustrates the desired combination of corporate signage and high quality building materials and design, as opposed to the example to the right, which utilizes the entire building a corporate “sign”, with little attention to building character or design.



No

The examples to the left illustrate two very different approaches to site advertising and corporate branding. The upper gas station calls attention to itself by liberally applying its corporate colors to virtually every surface of the site. The same effect is achieved in the lower gas station, albeit more tastefully, through the use of higher quality building elements that are repeated in both the main structure and the station canopy, and a consistent, yet restrained, use of the corporation’s colors throughout.



Yes

5. Multifamily Development

Intent: Multifamily development in the town plays an important role in supplying the housing needs of town residents, as well as establishing the character of the town. The higher density nature of multifamily housing represents an opportunity to create distinctive and high-quality living environments in the town. Their higher density also requires care that such living environments make a positive, long-term contribution to the town and continue to be attractive places to live for years to come. While many of the guidelines set forth above apply to multifamily development, the following additional guidelines for multifamily and large-scale single-family development are set forth as follows:

a. Connectivity

- Culs-de-sac are prohibited where connections between streets can be made, except where such connections cannot be reasonably made or do not serve the overall public good of the town. The public good served by connecting streets includes, but is not limited to: 1) The safety and convenience conferred on pedestrians and drivers by providing more connectivity and options for movement through the town; and 2) by building on the established character of the town.
- New multifamily housing shall be sensitively integrated into the fabric of the existing town, rather than consisting of isolated and self contained pods surrounded by surface parking.
- Traditional parking and circulation techniques, such as alleyways, shall be encouraged, to enable parking to be provided in the rear of residential structures.

b. Site Layout

- Housing shall be provided in concert with well defined, functional open spaces. Such spaces can vary in size and function, but should clearly address the public realm and serve as an organizing feature of the site.

- Community facilities such as pools, fitness facilities and clubhouses shall be located centrally, so as to be easily accessible by residents.
- Residential structures fronting along streets and rights-of-way shall include “public” components that actively address the street and public realm such as porches. Streets dominated by garages shall be avoided.
- Residential structures shall, wherever possible, “front” onto open space; that is to say, the public side of the residential structure shall be oriented toward open space, not the private, backyard side. The “public” sides of residential structures create a dynamic seam between open space and residential units, that also acts as “eyes on the park,” creating a sense of security for open space users.



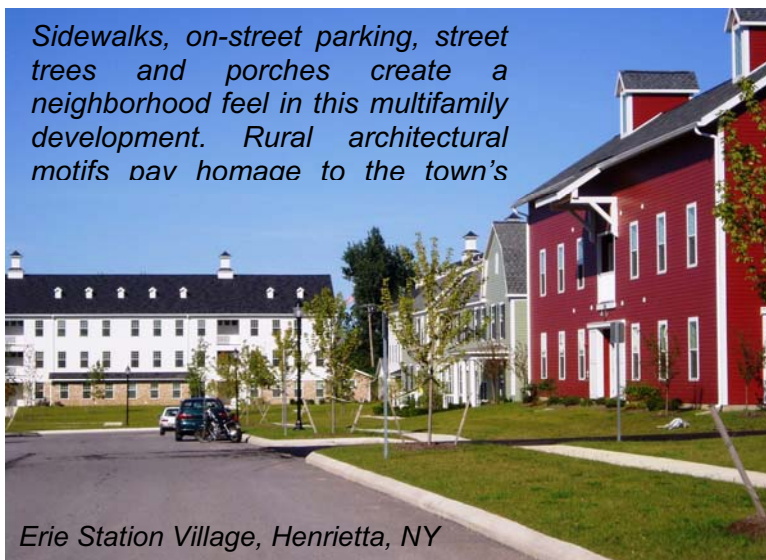
Portland, OR



Rochester, NY

Examples of successful pairings of open space and higher density housing.

Sidewalks, on-street parking, street trees and porches create a neighborhood feel in this multifamily development. Rural architectural motifs pay homage to the town's



Erie Station Village, Henrietta, NY

