

ORDINANCE NO. 17 – 09
City Center Code Changes and Rezone

WHEREAS, the City Council of the City of North Logan, Utah, adopted and passed the Code of Revised Ordinance of the City of North Logan on March 16, 1989, which Code was published and effective on March 20, 1989; and

WHEREAS, said Code included Title 12, Land Use, which is the City's Zoning Ordinance; and

WHEREAS, North Logan City Ordinance 04-05 implemented a city center code and zoning within the city that the North Logan Planning Commission and the City Council now deems in need of revision.

NOW, THEREFORE, pursuant to Section 12A-504 of Title 12 of said code and Section 10-9-205, Utah Code Unannotated, 2007, as amended, and after ten (10) days' notice and a public hearing as required therein, the City Council of the City of North Logan, Utah hereby adopts, passes and publishes the following:

AN ORDINANCE AMENDING TITLE 12C - LAND USE, ZONING ORDINANCE
OF THE CITY'S CODE OF REVISED ORDINANCES
BY AMENDING PORTIONS OF CODE RELATING TO THE
NORTH LOGAN CITY-CENTER CODE,
REVISING PORTIONS OF THE CODE
AND
AMENDING THE ZONING MAP ATTACHED THERETO BY
REZONING CERTAIN PARCELS

BE IT ORDAINED by the City Council of the City of North Logan, Utah as follows:

SECTION I: MIXED USE ZONING ORDINANCE: Title 12C, Zoning Ordinance, shall be changed as follows. Items deleted are crossed through, new language is highlighted gray..

12C-1060 North Logan City - City Center Code

12C-1060.00 Introduction

12C-1060.01 Applicability

12C-1061 The City Center

12C-1062 Regulating Plan

12C-1063 Zones

12C-1064 Complete Streets

12C-1065 Open Space Requirements

12C-1066 Building Form Requirements

12C-1067 Landscaping

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12C-1069 Retention And Maintenance Guarantees And Covenants

12C-1060.00 Introduction

The City Center Code is a legal document that regulates land development in North Logan City, Utah. This City Center Code was adopted to promote the health, safety and general welfare of residents of North Logan City.

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Adopted by Ord. 12-09 on 12/19/2012

12C-1060.01 Applicability

- A. Provisions of this Code are activated by "shall" when required; "should" when recommended; and "may" when optional.
- B. The provisions of this Code, when in conflict, shall take precedence over those of other North Logan City codes, ordinances, regulations and standards except the Local Health and Safety Codes.
- C. The existing Land-Use Ordinances of North Logan City, Titles 12A, 12B, 12C and 12D including the current Zoning and Subdivision Ordinances (the "Existing Local Codes") shall continue to be applicable to issues not covered by this City Center Code except where the Existing Local Codes would be in conflict with or are superseded by this City Center Code.
- D. Capitalized terms used throughout this Code are defined in 12C-1061.01
- E. Definitions. Terms not defined in Section 12C-1061.01 shall refer to their commonly accepted meanings. In the event of conflicts between these definitions and those of the Existing Local Codes, those of this Code shall take precedence except where usage and context requires otherwise.
- F. Standards and Tables are an integral part of this Code. However, the diagrams and illustrations that accompany them should be considered guidelines.

Adopted by Ord. 12-09 on 12/19/2012

12C-1061 The City Center

12C-1061.01 City-Center - Code-Specific Definitions

All definitions contained in the North Logan City Code, including the Land Use Titles and Chapters, shall apply to the interpretation of this City Center Code. Other terms as defined in the Utah Code, Section 10-9a-103 Definitions shall also apply. In the event of conflict between the following City Center Code Definitions and those contained in the North Logan City Code and/or the Utah Code, the Definitions contained in this City Center Code shall prevail, except to the extent the Definitions in the North Logan City Code clearly apply by use and/ or context and/or except to the extent the Utah Code Definitions apply as required by Utah law.

Adopted by Ord. 12-09 on 12/19/2012

12C-1061.01 City-Center - Code-Specific Definitions

The following definitions are defined for the City Center Code 12C-1060 through 12C-1069.

- 1. Arcade: A building frontage conventional for Retail use wherein the facade is a colonnade supporting habitable space that overlaps the sidewalk.
- 2. Avenue: A Thoroughfare of high vehicular capacity and low to moderate speed, acting as a short distance connector between urban centers, and usually equipped with a landscaped median.
- 3. Bicycle Lane: A dedicated lane for cycling within a moderate - speed vehicular Thoroughfare, demarcated by striping.
- 4. Bicycle Route: A Thoroughfare suitable for the shared use of bicycles and automobiles moving at low speeds.

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5. Block: The aggregate of private lots, passages, rear alleys and rear lanes, circumscribed by Thoroughfares.
6. Block Face: The aggregate of all the building Facades on one side of a Block.
7. Brick is to be "Standard" brick (3 5/8" x 2 1/4" x 8") or larger, and red or earth tone color.
8. Building Type: The general forms or types of building structures permitted in the City Center. (See Section 12C-1066).
9. Buffer Area: A specific area defined on the Regulating Plan for transitions from one type of Zone to another. The Buffer Area is not intended to mean the same as a no-build area. Construction can occur in this area if it complies with the applicable regulations
10. City: Referring to North Logan City.
11. City Center: In general it comprises the area within all the Zones of this Code, Sections 12C-1260 through 1269.
12. City Center Adjacent (CCA): A Zone in the City Center.
13. City Center Commercial (CCC): A Zone in the City Center.
14. City Commercial Building (CCB): A Building Type permitted in the City Center. (See Section 12C-1066).
15. Civic: The term defining not-for-profit organizations dedicated to arts, culture, education, recreation, government, or municipal use.
16. Civic Building: A building operated by not-for-profit organizations dedicated to arts, culture, education, recreation, government, transit, and municipal use, or for some other public use approved by the legislative body.
17. Civic Space: An outdoor area dedicated for public use. Civic Space types are defined by the combination of certain physical constants including the relationships among their intended use, size, landscaping and spaces defined by buildings.
18. Classic Commercial Building (CCB): A Building Type permitted in the City Center. (See Section 12C-1066).
19. Common Space: Open Space that belongs to the development, HOA or an organization. The land use often includes but not limited to open lawn or grass areas, landscaping, courtyards, recreation areas, walks, trails and other types of common areas.
20. Common Green: A type of Open Space that is required for developments with a minimum certain number of units. (See Section 12C-1065).
21. Density: The number of Dwelling Units permitted per acre.
22. Design Review Committee: Committee assigned by the Mayor to review the design of all projects within the City Center.
23. Design Speed: The velocity at which a Thoroughfare tends to be driven without the constraints of signage or enforcement.
24. Development Plan and Agreement: A specific plan for the development of a certain property showing buildings, streets, walks, Open Space, parking and landscaping. Includes documentation stating any agreement between the developer and the City relative to the development.
25. Downtown: The general commercial and or civic core center of the City, typical to most cities.
26. Downtown Zone (DT): A Zone in the City Center.
27. Dwelling Unit: Consists of one or more rooms that contain living facilities, including provisions for sleeping and eating, cooking and sanitary facilities, inhabited by one or more persons living together and maintaining a common household, in a residential building or residential portion of a building.

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28. Euclidian Zoning: Land use codes characterized by the segregation of land uses into specified geographic districts and dimensional standards stipulating limitations on development activity within each type of district.
29. Frontage: The area between a building Facade and the vehicular lanes, inclusive of its built and planted components. Building Frontages can be public or private.
30. Frontage Building: A commercial building that is located on the street side of a parking structure.
31. Form-based Codes: Land use codes which focus on the physical form of the community such as buildings, blocks, walks, streets, setbacks, open space, landscaping and less emphasis on conventional separate uses. Form-based Codes address the relationship between building facades and the public realm. The regulations and standards in Form-based Codes are presented in both words and clearly drawn diagrams and other visuals.
32. Gallery: A Private Frontage conventional for Retail use wherein the Facade is aligned close to the Frontage with an attached cantilevered shed or lightweight colonnade overlapping the Sidewalk.
33. Garages (G): A Building Type permitted in the City Center. (See Section 12C-1066).
34. Green: A Civic Space type for unstructured recreation, spatially defined by landscaping rather than building Frontages.
35. Greenway: An Open Space corridor in largely natural conditions which may include trails for bicycles and pedestrians.
36. Gross Acreage: The total sum of the area within a parcel being developed, or an individual Lot. Can also refer to the acreage of an individual lot.
37. In fill: New development on land that had been previously developed, including most grey field and brown field sites and cleared land within urbanized areas.
38. Lodging: Premises available for daily and weekly renting of bedrooms.
39. Lot. A parcel of land occupied or to be occupied by a main building, or group of buildings (main and accessory) together with such yards, open spaces, lot width and lot area as are required by existing local codes and/or by this City Center Code and having frontage upon a street except when exempted in the approved Development Plan and Agreement.
 - a. Except for group dwellings, not more than one (1) dwelling structure shall occupy any one (1) lot, unless otherwise provided in the approved Development Plan and Agreement.
 - b. A fee simple parcel which complies with the requirements of existing local codes and/or this City Center Code for subdivisions or approved developments shall be deemed a "lot" for the purposes of this ordinance.
 - c. A lot shall also mean a portion of a subdivision intended as a unit for development or transfer of ownership.
40. Medium Single Family (MSF): A Building Type permitted in the City Center. (See Section 12C-1066).
41. Mixed Residential 8 or 7 (MR 8 or MR 7): Zones in the City Center.
42. Mixed-Use: A mixture of different uses on the same lot or within close proximity. Often commercial and residential dwelling units mixed.
43. Multi-Unit Building (MUB): A Building Type permitted in the City Center. (See Section 12C-1066).
44. ~~Multi-Unit House (MUH): A Building Type permitted in the City Center. (See Section 12C-1066).~~

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45. Open Space: Natural areas or land developed as recreational space. Recreational open space often includes trails, grass areas, sports fields, squares, plazas, greens, courtyards, and landscaping. Required Open Space for development is defined in Section 12C-1065. (Open Space Areas – See Section 12C-1069)
46. Parking: The act or practice of temporarily leaving a vehicle or maneuvering a vehicle into a certain location deemed appropriate by the City.
47. Parking Structure: A parking garage or elevated parking lot designed to park cars on multiple levels.
48. Pedestrian Shed: An area that is centered on a common destination. Its size is related to average walking distances to nearby destinations and availability of sidewalks/trails. A Pedestrian Shed is commonly an area within a distance 1/4-1/2 mile distance or within 15-20 minutes.
49. Parapet Wall: An outside wall on a building generally with a flat roof. The parapet wall extends up from the top of the roof.
50. Planter (or Park Strip): Space between the sidewalk and the curb/gutter. In neighborhoods it contains vegetation and trees and in urban settings it contains trees, lighting and street furniture.
51. Plaza: A Civic Space type designed for Civic purposes and commercial activities in the more urban areas, generally paved and spatially defined by building Frontages.
52. Principal Entrance: The main point of access for pedestrians into a building.
53. Private Open Space: An outdoor space or spaces including such things as play areas and/or landscaping, or other space held in common by separate owner(s) within an HOA or owned by a single Landowner, and where the HOA/ owner has full responsibility for maintenance and liability of the space.
54. Regulating Plan: A Zoning Map or set of maps specific to this City Center Code that shows the Zones, Civic Zones, Special Zones if any, and transportation network.
55. Residential Density: See Density.
56. Retail: Characterizing premises available for the sale of merchandise and food service.
57. Retail Frontage: Frontage designated on a Regulating Plan that requires or recommends the provision of a Shop Front, encouraging the ground level to be available for Retail use.
58. Single Family 6 (SF6): A Zone in the City Center.
59. Shop Front: A Private Frontage conventional for Retail use, typically with substantial glazing and an awning, wherein the Facade is aligned close to the Frontage of the building entrance at Sidewalk grade.
60. Square (see also Town Square): A Civic Space type designed for unstructured recreation and Civic purposes, spatially defined by building Frontages and consisting of Paths, lawns and trees, formally disposed.
61. Steel: To be left “raw” or if colored must be powdercoated black or earthtone.
62. Stoop: A Private Frontage wherein the Facade is aligned close to the Frontage with the first Story elevated from the Sidewalk for privacy, with an exterior stair and landing at the entrance.
63. Story. A space within a building included between surface of any floor and the surface of the ceiling next above.
64. Timeless: Design or architecture that is relevant for several generations, it outlast trends, fashion statements and society's infatuation with styles. It is made of materials and methods that will last a century or more.
65. Thoroughfare: A way for use by vehicular and pedestrian traffic and to provide access to lots and Open Spaces, consisting of vehicular lanes and the Public Frontage.

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66. Town Square: The public Square or Civic Green of the City. A place for public events and art shows, passive recreation and gathering. (See Townsquare (TS) on the Regulating Plan, Section 12C-1062)
67. Townhome (TH): A Building Type permitted in the City Center. (See Section 12C-1066). Townhouse and Townhome can be used interchangeably.
68. Traditional Neighborhood Development (TND): A community unit type characterized by a Pedestrian Shed oriented toward a common destination consisting of a village or city center.
69. Trail(s): A path used for transit or recreation by pedestrians or cyclists.
70. Two-Family Home (TFH): A Building Type permitted in the City Center. (See Section 12C-1066).
71. Urban: Collective term for the condition of a compact, Mixed-Use settlement, including the physical form of its development and its environmental, functional, economic, and socio-cultural aspects.
72. Walkable: The ability for a pedestrian/wheelchair to safely walk through the City Center and neighborhoods. Walkable communities have properly sized blocks (See Section 12C-1064), sidewalks, crosswalks, ramps and are built with the pedestrian in mind. Walkable communities provide the ability to walk to daily needs within a comfortable walking distance, generally 1/2 mile or less.
73. Zone(s): A geographic area or district which is planned to have common ordinances for its development and planning.
74. Zoning Map: The official map or maps that are part of the zoning ordinance and delineate the boundaries of individual Zones and districts. (See Regulating Plan)

Adopted by Ord. 12-09 on 12/19/2012

Amended by Ord. 16-06 on 9/7/2016

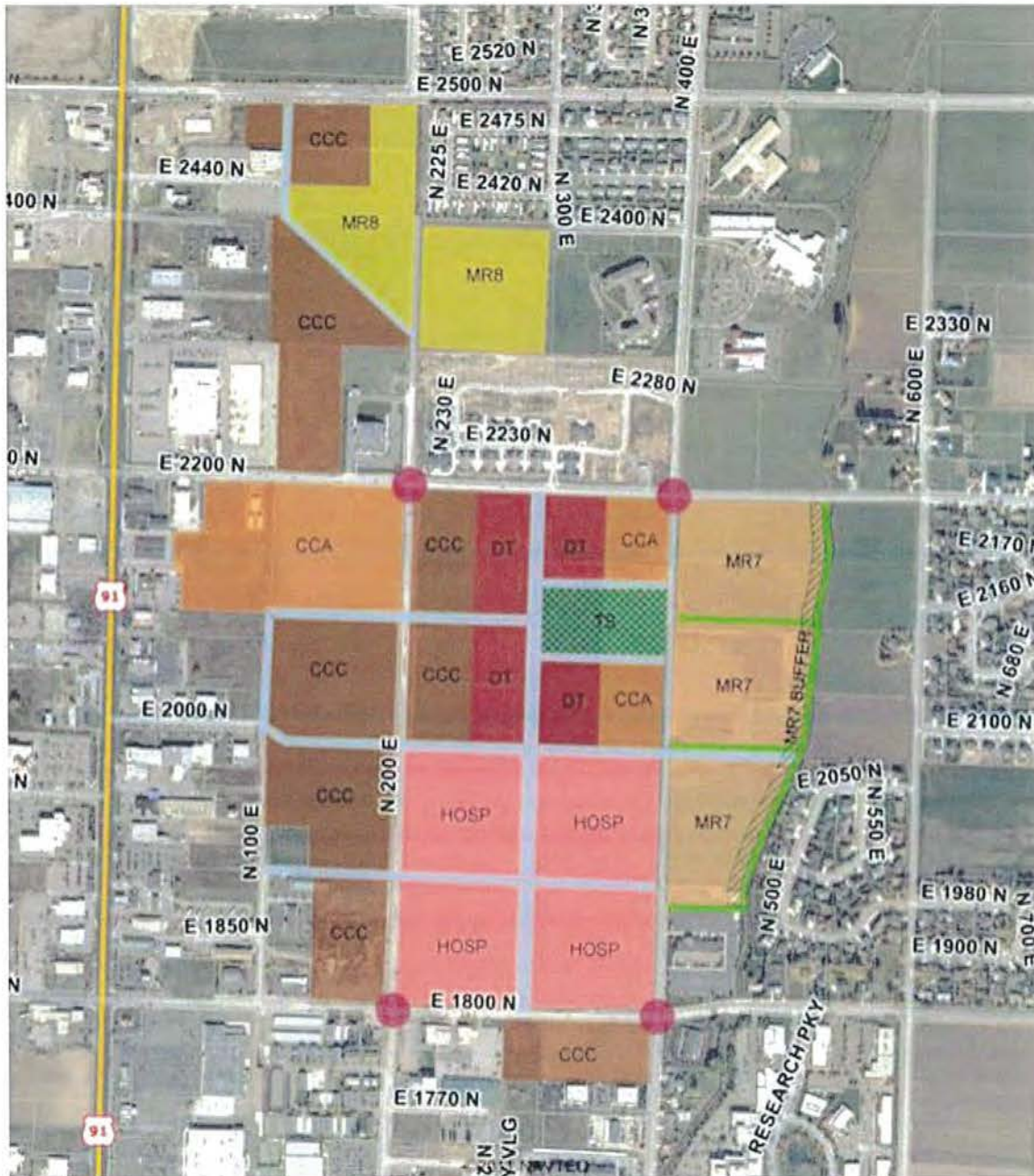
12C-1062 Regulating Plan

12C-1062.01 Regulating Plan Descriptions

12C-1062.02 City Center Zones Descriptions

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- | | |
|------------------------------|------------------------|
| City Center Commercial - CCC | Future Townsquare Land |
| City Center Adjacent - CCA | Future Street ROW |
| Downtown - DT | Round About |
| Mixed Residential 8 - MR8 | Multi-use Trail |
| Mixed Residential 7 - MR7 | MR7 Buffer |
| Hospital - HOSP | |



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12C-1062.01 Regulating Plan Descriptions

A. The Regulating Plan divides the City Center into Zones. These Zones identify areas of future neighborhood and district types.

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- B. The Zones allocate the location of building forms, building frontages, and other development considerations to create the desired design standards for each.
- C. The conceptual area boundary indicates a required arrangement of Zones and conceptual street layout for areas.
- D. Modifications to Zone boundaries can be made through the process of amending the City's Land Use Code (See Title 12C Zoning). Acceptable circumstances for modifications to the Regulating Plan would include the following:
 - 1. Modifications to the street standards plan and transportation plan.
 - 2. Protection of natural features not previously identified.
 - 3. Revisions to the Regulating Plan.

Adopted by Ord. 12-09 on 12/19/2012

12C-1062.02 City Center Zones Descriptions

- A. Downtown – DT: The intent of this Zone is to provide for the Commercial and Civic core of the City Center. Downtown Zone is planned to look and function like a historic main street. The DT Zone will have the most urban feel. The DT Zone is planned to be Mixed-Use with commercial (office, retail, restaurants and more), residential (permitted on upper stories only), and civic greens. Street Parking, civic green space and wide sidewalks will encourage the vibrant City Center activity. The DT has a network of streets with wide sidewalks, street lighting, consistent street tree plantings and buildings set close to the sidewalks. DT requires that commercial uses be located on ground level at minimum.
- B. City Center Adjacent – CCA: The intent of this Zone is to provide an area of higher density Mixed-Use developments/buildings that accommodate retail, offices, townhomes and apartments. The CCA Zone will have the highest residential Density with condominiums, apartments, townhomes and other housing options. One of the CCA's main roles is to support the City Center economy by providing housing opportunities and a population within a walkable distance of the Downtown. The CCA has a network of streets with wide sidewalks, street lighting, consistent street tree planting and buildings set close to the sidewalks.
- C. Mixed Residential – MR8 & MR7: These zones are primarily residential in the form of a Traditional Neighborhood Design. MR7 and MR8 may have a wide range of Building Types: single-family residential, two-family residential, townhomes and others. Setbacks and landscaping are variable. Streets with curbs and sidewalks define medium-sized blocks. Street trees and street lighting will also help define the neighborhoods and add character. There are two significant differences between MR8 and MR7, specifically allowed density, Building Types permitted and how MR7 lots buffer single family lots.
- D. Single Family Residential – SF6: SF6 is designed as a higher-density single family area. This zone is residential in nature and is intended to be in the form of a Traditional Neighborhood Design. Building Types are limited to small and medium single family homes, though variation in floorplan and appearance is required throughout a project. It is designed for a higher-density single family area. As with the MR7 and MR8 zones, streets will have curbs and sidewalks to define blocks. Street trees and street lighting will also help define neighborhoods and add character.
- E. City Center Commercial – CCC Zone: This zone is to provide a transition Zone between the City Center and US Highway 91 commercial and provide an opportunity for mid-box commercial and Mixed-Use office. The CCC will primarily be commercial buildings with

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slightly wider lots than CCA or DT and with a wider variety of setbacks. CCC Zone Parking shall be on the side or rear of the lots/building to encourage walkability and complement the feel and architecture of the City Center. The CCC will also have sidewalks, connecting streets and street lighting.

- F. Hospital – (HOSP): Zone is to provide a process by which a Hospital campus and series of medical buildings could be planned and designed on property owned by IHC at the time of the passage of this ordinance, and to do so in accordance with the City Center Code requirements.

Adopted by Ord. 12-09 on 12/19/2012

12C-1063 Zones

12C-1063.01 Requirements Common To All Zones

12C-1063.02 Thoroughfares/Complete Streets

12C-1063.03 Parking

12C-1063.04 Open Spaces

12C-1063.05 Development Plan And Agreement

12C-1063.06 Building Codes

12C-1063.07 Storm Water Drainage

12C-1063.08 Residential Density Table

12C-1063.09 Buildings And Lot Plat Requirements

12C-1063.10 Downtown (DT) - Specific Requirements

12C-1063.11 City Center Adjacent (CCA) - Specific Requirements

12C-1063.12 Mixed Residential 7 And 8 (MR7 And MR8) - Specific Requirements

12C-1063.13 City Center Commercial (CCC) - Specific Requirements

12C-1063.14 Hospital Zone (HOSP) City-Center - Specific Requirements

12C-1063.01 Requirements Common To All Zones

All properties identified as City Center Code and Regulating Plan 12C-1062 shall adhere to the following guidelines:

- A. Each City Center Development Plan shall respond to the existing conditions of the site, adjacent developments, connecting Thoroughfares, and natural features.
- B. All development projects are to be reviewed and approved for compliance to the City Center Design Criteria by the Design Review Committee.
- C. Each City Center Development Plan shall be structured as a walkable neighborhood with a priority of connecting to the City commercial area (300 East).
- D. Each City Center Development Plan shall lay out the Thoroughfare network according to Complete Streets 12C-1064 and City Standards.
- E. Each City Center Development Plan shall allocate Open Space according to 12C-1065.
- F. Each City Center Development Plan shall include a set of building plans prepared in accordance with the requirements in the Building Forms Requirements 12C-1066, all other applicable North Logan City building ordinances/codes and International Building Codes (IBC)
- G. Each City Center Development shall include a set of accurately drawn 3D renderings of the plan and materials board. Artist drawings shall be sufficient for this requirement.
- H. Each City Center Development shall include a set of accurate 3D renderings. Renderings shall represent building's massing and materials as well as proposed signage. Artist sketches shall not be sufficient for this requirement.

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- I. Each city center development shall have articulated building forms. This includes, but is not limited to, a requirement that massing, walls, and roofs, have appropriately scaled modulations that contribute to the development of visually aesthetic and well articulated building designs. This pertains to all elevations of a building.
- J. The City Center Code and the Building Types in these Zones do not prescribe any specific architectural style. Massing diagrams and image examples used represent the desired form, massing, and qualities for development in the City Center. All projects will be evaluated in light of the representations in the benchmark images in this document.
- K. Each City Center Development Plan shall include a set of landscape plans conforming to 12C-1067.
- L. Each City Center Development Plan shall include a street tree master plan that is in accordance with North Logan City landscaping requirements 12C-1067.
- M. Each City Center Development Plan shall include all required architecture and civil engineering plans as required by North Logan City.
- N. All proposed signs to be included in a City Center Development Plan shall conform to Section 12C-600 and, where applicable, require a sign permit.
- O. All buildings in the City Center must be roofed with roofing material graded as lasting 30 years or longer. All membrane roofs must be hidden by parapets or other appropriate techniques as approved by the Design Review Committee and the City Council.

Adopted by Ord. 12-09 on 12/19/2012

12C-1063.02 Thoroughfares/Complete Streets

- A. Thoroughfares are intended for use by vehicular, bus transit, bicycle and pedestrian traffic and to provide access to lots and open spaces.
- B. The Thoroughfares consist of vehicular lanes and streetscapes as defined in the Complete Streets Section 12C-1064. The lanes provide the traffic and parking capacity. They consist of vehicular lanes in a variety of widths for parked and for moving vehicles. Streetscapes or street frontages contribute to the character of the Zone. They include specified types of sidewalks, curbing, planters, and street trees.
- C. Thoroughfares should be designed in context with the urban form and desired design speed of the Zones through which they pass.
- D. Within the DT, CCA, and CCC Zones, pedestrian comfort and safety shall be primary considerations for the Thoroughfare design in the more urban or city setting.

Adopted by Ord. 12-09 on 12/19/2012

12C-1063.03 Parking

- A. Parking requirements shall be according to the building type/Zone as shown in the Building Forms Requirements Section 12C-1066.
- B. Each Development Plan must show a table listing Parking requirements, Parking stalls on plan and required bike Parking.
- C. The use of bio-swales and on-site retention is encouraged for Parking lot drainage and stormwater requirements.
- D. All off-street Parking lots must have landscaping within the Parking lot area. For every fifteen (15) Parking stalls there shall be the equivalent of one (1) stall space landscaped with water wise

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plantings and one (1) shade tree. The purpose of the landscaping area within the Parking lot is to minimize urban heat island effect, beautify Parking areas, and offer on-site stormwater solutions. Landscaping areas must be planted with trees and comply with Landscaping section 12C-1067.

- E. All off-street Parking lots must be set back from street curb a minimum of fifteen (15) feet and buffered by landscaping from the street.

Adopted by Ord. 12-09 on 12/19/2012

Amended by Ord. 16-06 on 9/7/2016

12C-1063.04 Open Spaces

- A. All plans and developments must comply with the Open Space Requirements, Section 12C-1065.
- B. All proposed projects and plans must include an Open Space plan.

Adopted by Ord. 12-09 on 12/19/2012

12C-1063.05 Development Plan And Agreement

- A. The Developer/Project Applicant shall be required to comply with the current City Code provisions for the Development Plan and Agreement, including North Logan City Code Subsection 12C-1004, Zoning, Modifying Regulations; Chapter 12C-1050, Mixed Use Ordinance; Chapter 12C-1100, Zoning-Site Development Standards; Title 12D, Land-Use-Subdivisions, and all other applicable building code, permitting and other provisions of the current City Code, and as it may be amended, to the extent not repealed or superseded by the provisions of the City CenterCode.
- B. In addition to complying with existing City Code and City Center Code requirements, the Developer/Project Applicant will be required to attend a Public Hearing held by the City Council when the project is proposed for concept approval. After the Planning Commission has reviewed the project and provided recommendations, the City Council will hold a public hearing to review the proposed concept plan of the project. Fourteen (14) days prior to the hearing the developer/proponent must deliver a digital copy to the City for posting on the City's Official website and the Developer/Project Applicant shall post and maintain a minimum size 24"x36" weather resistant print/board of the concept plan on the project property for public notice and inspection. The print/board must show the proposed conceptual plan of the entire project proposed. The posted board shall be posted adjacent to the nearest street or road to the project in a conspicuous place on the project property that is reasonably calculated to give notice to passers-by. Notice of the Public Hearing shall also be given as required by City Code 12A-504, and as updated by Utah Code 12-9a-204.
- C. The development plan must also show adjacent landowners and existing or anticipated uses on adjacent properties and detail any opportunities to cooperate on common-use Parking, landscaping, stormwater retention, access, etc.
- D. The project shall provide the City with the required documents for HOA and maintenance plan according to Section 12C-1069.

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12C-1063.06 Building Codes

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The Developer/Project Applicant shall be responsible for providing the needed drawings and adhering to all of the City’s building codes per North Logan City Code, Title 9. Building and Construction, all other applicable federal, state and local building codes, regulations and laws, and according to this City Center Code.

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12C-1063.07 Storm Water Drainage

- A. All stormwater shall be retained on site in accordance with City and State regulations.
- B. The developer shall follow and adhere to all local and state stormwater code requirements including but not limited to: providing and following a Stormwater Pollution Prevention Plan (SWPPP), stormwater inspections by a Registered Stormwater Inspector (RSI), appropriate stormwater BMPs, low impact handbook design standards, and providing grading and drainage civil engineering plans.
- C. The stormwater facilities and areas that are located within the City-owned and dedicated land shall ~~may~~ be maintained by the City and storm water facilities on private property shall be maintained by the owner unless otherwise provided for as part of the Development Agreement.

Adopted by Ord. 12-09 on 12/19/2012

12C-1063.08 Residential Density Table

Zones	Minimum Density	Maximum Density
Downtown, DT	0	8
City Center Commercial, CCC	0	4
City Center Adjacent, CCA	0	12 east of 200 East* and 18 west of 200 East*
Mixed Residential 8, MR8	6	8
Mixed Residential 7, MR7	4	7
City Center Residential Single Family 6, <u>SF6</u>	4	6

* 200 East here means 200 East from 2200 North to the south and Wolf Pack Way from 2200 North to the north.

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Amended by Ord. 16-06 on 9/7/2016

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12C-1063.09 Buildings And Lot Plat Requirements

- A. The official position of the City is to recommend and encourage the development of residential dwellings in the City Center, with the majority of such residential dwellings being owner-occupied. City Officials and staff are directed to work with each Developer/Project Applicant to accomplish this objective and to preserve the owner-occupied status through the use of recorded Covenants, Conditions & Restrictions (CC&Rs), recorded Restrictive Covenants and other mutually agreeable methods.
- B. The following Building Types in all Zones shall have individually platted lots per units: Small Family Home, Medium Family Home, Two Family Home, and Townhome.

Adopted by Ord. 12-09 on 12/19/2012

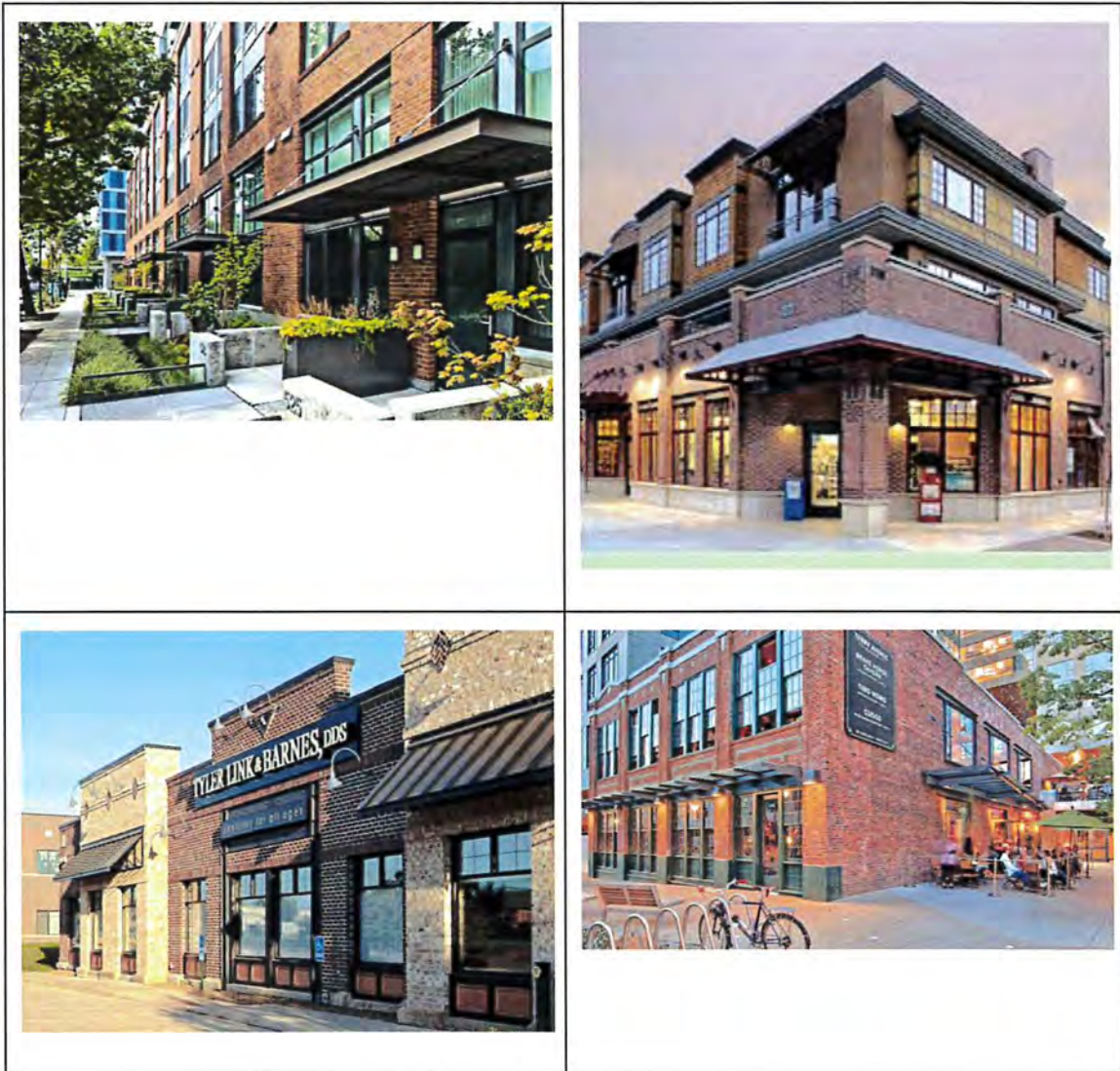
12C-1063.10 Downtown (DT) - Specific Requirements

The intent of the Downtown Zone is such that the area should be planned to look and function like a historic main street. The DT Zone will have the most urban feel. The DT Zone is planned to be Mixed-Use with commercial (office, retail, restaurants and more), residential Dwelling Units only located on upper stories (~~optional, but not required~~), and a prominent Town Square. The Town Square location can be identified on the Regulating Plan. (See Section 12C-I 062). Street Parking and wide sidewalks will encourage the vibrant City Center activity. The DT has a network of streets, with wide sidewalks, consistent street tree plantings and buildings set close to the sidewalks. DT Zone requires that commercial uses be located on the main floor level and they may also be located on second and in some cases a third floor. The DT Zone is designed to be flexible to market conditions allowing for a mixture of professional services, entertainment, office, retail and restaurants. The public frontage will require wider sidewalks for sidewalk sales, outdoor cafe style eating, bike parking, street lights, benches and other street furniture as needed.

- A. Examples:DT



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- B. All plans for proposed projects must conform to the City Center Code 12C-1060 through 12C-1069 and all other applicable North Logan City ordinances. For the Downtown (DT) Zone as shown on the Regulating Plan and City Zoning Map, the following Special Requirements, and standards shall be applied:
- C. Building Frontage:
1. The street facade of the buildings should provide a Shop Front appearance at the sidewalk level. The Shop Front shall consist of all building facades facing the primary street. The Shop Front shall be no less than 60% glazed in clear glass (not a requirement on 2nd or 3rd floors). It is recommended that the Shop Front be shaded by an awning overlapping the sidewalk. If the first story is built as office it is recommended that it be designed such that is would be flexible enough to serve as retail in the future as market conditions change.

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2. The developer/builder should consider using a Gallery, Awning, Arcade or Retail type frontage. The intent of this Zone and area is to create a unique and enjoyable outdoor commercial retail area

D. Density:

1. Residential Density is permitted and encouraged but not required in the DT Zone; however Dwelling Units are only permitted on the second and third floor in the DT Zone. (See Section 12C-1063.08).
2. The DT Zone requires commercial uses at the main floor level but residential is permitted on the second and/or third floor. All residential units above the main floor require a separate access separate from the entrance to the commercial use on the 1st floor of the building.
3. All residential units must meet the off-street parking requirements per building and Zone requirement as shown in the Building Forms Requirements Section 12C-1066.

E. Streets and Blocks:

1. Streets and blocks will conform to Section 12C-1064.
2. Six-foot (6') wide pedestrian Mid-Block crossing walks are required in the DT Zone for Blocks Faces longer than 330 feet per side.
3. Mid-Block crossings should be of a different material than street construction materials. The crossings shall be painted and receive signage according to AASHTO regulations.
4. In the DT Zone one courtyard per block face is permitted and encouraged. The building setback for the courtyard is a maximum of 100 feet with only one courtyard allowed per Block Face. The courtyard must be landscaped and or hardscape in the plaza. Building frontages and front doors shall face either the courtyard or the street.

H. Building Materials:

1. High-quality exterior finishes must be used on all sides of buildings in the DT zone. As high quality exterior finish materials are defined as brick, fiber-cement or similar lap siding, glass, unpainted sandblasted concrete, steel, or real stone. Brick is to be used on 75% (minimum) of all non-glass areas of each facade. All awnings are to be metal or glass (no fabric).

Adopted by Ord. 12-09 on 12/19/2012

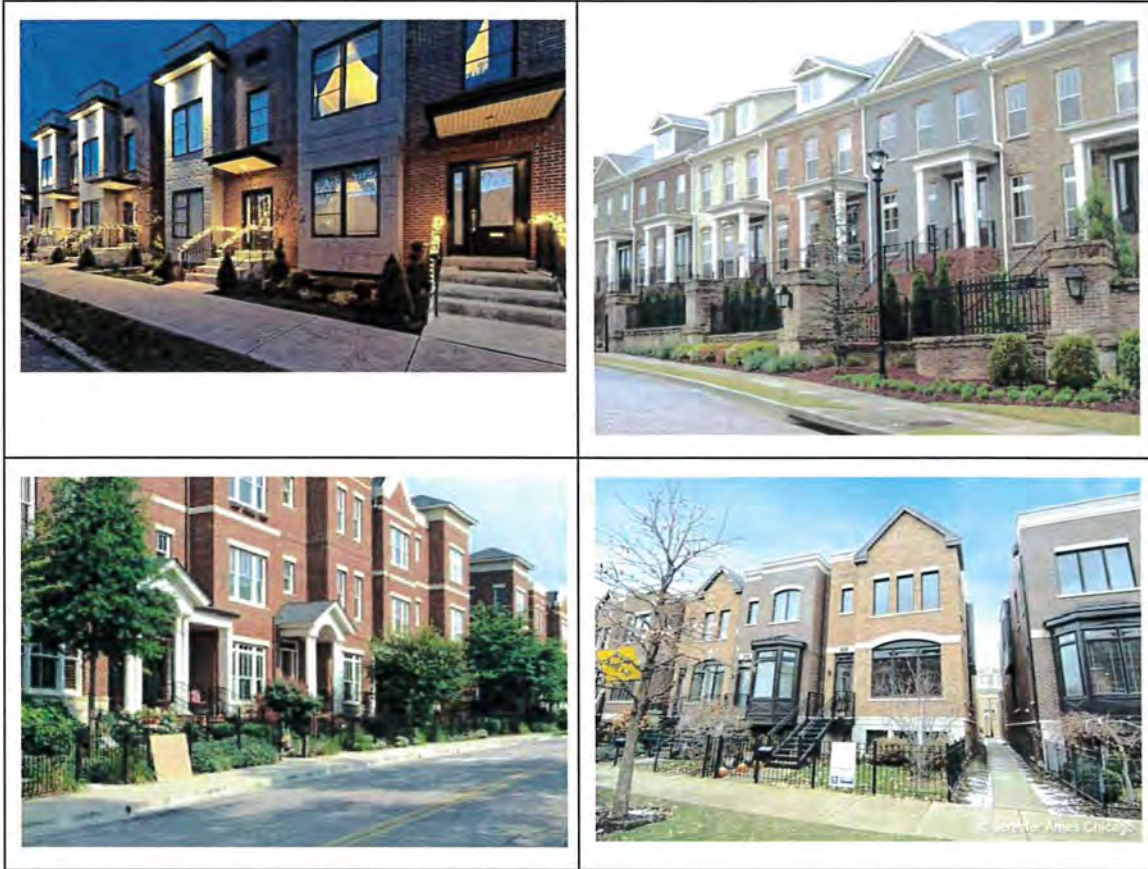
Amended by Ord. 16-06 on 9/7/2016

12C-1063.11 City Center Adjacent (CCA) - Specific Requirements

The intent of the City Center Adjacent (CCA) Zone is to offer Mixed-Use opportunity in higher density Mixed-Use buildings and/or retail, offices, townhouses, condominiums and apartments. The CCA Zone should have the highest residential density with apartments, townhouses and other housing options. One of the CCA's main roles is to support the City Center economy by providing a variety of housing options within the City Center. The CCA will be connected and within walking distance of the DT. The CCA has a network of streets, wide sidewalks, consistent street tree planting and buildings within shorter setbacks. The CCA Zone provides a Mixed-Use option in commercial and residential housing.

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A. Examples: CCA



- B. All plans for proposed projects must conform to the City Center Code 12C-1060 through 12C-1069 and all other applicable North Logan City ordinances.
- C. For the City Center Adjacent (CCA) Zone shown on the Regulating Plan, the following Special Requirements and standards shall be applied:
- D. Building Forms and Frontages:
 - 1. Building Types shall be according to Section 12C-1066.
 - 2. The CCA Zone is primarily residential housing within the City Center and designed to create the urban neighborhood feel. Apartments, condominiums, townhomes and commercial buildings have a closer proximity to the street to develop a quality public realm feel on the sidewalk.
 - 3. The CCA is a Mixed-Use type of zone with both residential and commercial uses permitted. If Commercial buildings are developed in the CCA, the street facade of the buildings should provide a Shop Front appearance at the sidewalk level. The Shop Front shall consist of all building facades facing the primary street. The vertical height on the

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Shop Front shall be from ground level up to a minimum of 10 feet vertical height. The Shop Front shall be no less than 50% glazed in clear glass. If the first story is built as office it is recommended that it be designed and flexible to serve as retail in the future as market conditions may change.

4. High-quality exterior finishes must be used on all sides of buildings in the CCA zone. High quality exterior finish materials are defined as brick, fiber-cement board or similar lap siding, glass, steel, (type of real stone or wood timber, certain types of decorative concrete, as a smaller percentage. Brick is to be used on 50% (minimum) of all non-glass areas of each facade. All awnings are to be metal or glass (no fabric).
 5. Density and Mixture:
 6. See Section 12C-1063.08 for Density Table.
 7. Density shall include a blend of housing types within the development plan. The variety of housing types will provide NLC residents with options and establish neighborhoods with various architecture styles and Building Types.
 8. In the CCA Zone developments that include residential uses, those developments shall have a minimum of two housing types. If the project has Dwelling Units it shall have a minimum of two (2) Building Types with a minimum percentage of thirty percent (30%) units of each of the two (2) types. The housing types shall come from the two lists (below) with a minimum of one (1) housing type coming from each list. If the project is built in two phases the housing mixture shall be averaged over the two projects. Building Types are defined in Section 12C-1066.
 - 9.
 10. CCA Building Type List 1 CCA Building Type List 2
 11. Multi-Unit Building ~~Two-Family Home~~
 12. ~~Multi-Unit House~~ Townhome
 13. Vertically Combined Commercial/Residential Building
 - ~~14.~~
 - 15.
 16. Density in the CCA Zone is per Gross Acreage developed as residential. For example if the total lot is 10 acres and if 5 acres are developed as residential and 5 acres as commercial, the Density shall be 5 acres times the Density number per Zone according to 12C-1063.08 Density Table. The acreage (including parking and open space) that is commercial only does not count towards Density. Commercial/Residential Combined areas do count towards the allowed density.
 17. The variation of Building Types as required in Section 12C-1063.10 F (3) (above) shall be mixed throughout the project.
- E. Streets and Blocks: Streets and blocks will conform to Section 12C-1064.

Adopted by Ord. 12-09 on 12/19/2012

Amended by Ord. 16-06 on 9/7/2016

12C-1063.12 Mixed Residential 7 And 8 (MR7 And MR8) and Single Family Residential 6 (SF6) Specific Requirements

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- A. City Center Residential Zones are primarily residential in the form of a Traditional Neighborhood Design. Permitted Building Types in the Mixed Residential Zones (MR7 and MR8) include: Small Single Family, Medium Single Family, Two-family, and Townhomes and Multi-Unit House. Permitted Building Types in the SF6 zone are small single family and medium single family only. Setbacks and landscaping are according to building type and Zone. The streets are defined by curbs, sidewalks, block sizes, street trees, and lighting. All requirements apply to both MR7 and MR8 and SF6 unless designated to apply specifically to one Zone, MR7, MR8 and SF6.

Examples: (SF6, MR7 & 8)



Click on photo to enlarge.



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- B. The Development Plan and Agreement for proposed projects must conform to the City Center Code 12C-1060 through 12C-1069 and all other applicable North Logan City ordinances.
- C. For the Mixed Residential Zones (MR8 & MR7) shown on the Regulating Plan, Special Requirements and standards shall be applied as follows:
- D. Building Forms and Frontages:
 - 1. The MR8 and MR7 Zones are designed to develop Traditional Neighborhood Design (TND). Local examples of TNDs can be found in historic neighborhoods of Logan City, Salt Lake City (Sugarhouse and Avenues) and other historic neighborhoods in Utah. Buildings in the MR Zones should have porches, the main entry, or green spaces facing the street. The MR Zones should foster a majority of owner occupied dwellings. Building Types shall be according to Section 12C-1066.
- E. Residential Density and Mixture:
 - 1. See Section 12C-1063.08 for Residential Density Table.
 - 2. Residential Density shall include a blend of a housing types within the development plan. The variety of housing types will provide NLC citizens with options and establish neighborhoods with various architecture styles and Building Types.
 - 3. In the MR Zones developments shall have a minimum of two building or housing types. The housing types shall come from the two lists (below) with a minimum of one (1) housing type coming from each list. Building Types are defined in Section 12C-1066.

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4. The Development Plan within the MR 7 zones shall have a minimum of two (2) Building Types split as equally as possible between two housing types selected from each of the following Lists 1 and 2. Provided that if an odd number of units can be allowed, one building type may be include one more dwelling unit than the other type.
5. Qualifying Senior Communities shall have a minimum percentage of forty percent (40%) units from each of the following Building Type Lists, both List 1 and List 2; or they may be all Two-Family Senior Homes. Furthermore, all Senior Housing dwelling units shall also have a height limit of no more than twenty-four feet (24') measured from the midpoint of the roof to finished grade.

MR7 Building Type List 1 MR7 Building Type List 2

Small- Single Family Townhome

Medium-Single Family Two-Family Home

Two-Family Senior Home*

* Two-Family Senior Homes are the same design type as Two-Family Homes but must be designated as Senior Housing. Two-Family Senior Homes may be counted as qualifying within the Type List 1 if the homes are designated Senior Housing and comply with the definitions of Senior Housing and Qualifying Senior Community in 12A-200 and with the requirements in the Zoning Matrix, Paragraph 12C-1001 (L).

6. The Development Plan within the MR 8 zones shall have a minimum of two (2) Building Types with a minimum percentage of thirty percent (30%) units from each of the following Lists 1 and 2.

MR8 Building Type List 1 MR8 Building Type List 2

Small-Single Family Townhome

Medium-Single Family ~~Multi-unit Home~~

Two-Family Home

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City Center Code Changes and Rezone

7. The variation of Building Types as required in Section 12C-1063.11 F(3) (above) shall be mixed throughout the project. The project shall not have a continuous use of one single Building Type on a Block Face.
8. The construction of Garages are regulated in each Zone as shown in Section 12C-1066.
- F. In the MR7 Zone, a Buffer Area is identified on the Regulating Plan in 12C-1062. This buffer is a 70- foot wide area running along the west of the Logan North Fields Canal. All lots extending into the Buffer Area shall contain only Small Single Family, Medium Single Family Houses, or designated Senior Housing Two-Family homes.
- G. Streets and Blocks. Streets and blocks shall conform to Section 12C-1064.
- H. For the City Center Residential Single Family Zone (SF6) shown on the Regulating Plan, Special Requirements and standards shall be applied as follows:
 - I. Building Forms and Frontages:
 1. The SF6 zone is designed to develop Traditional Neighborhood Design (TND). Local examples of TNDs can be found in historic neighborhoods of Logan City, Salt Lake City (Sugarhouse and Avenues) and other historic neighborhoods in Utah. Buildings in the SF6 Zones should have porches and the main entry facing the street. Building Types shall be according to Section 12C-1066.
 - J. Building Materials:
 1. Buildings in the MR7, MR8 and SF6 zones must have high quality exterior finish materials on all sides. High quality exterior finish materials are defined as brick, fiber-cement board or similar lap siding, glass, or stone.
 2. In MR7 and MR8 brick or stone must be used on 50% (minimum) of all non-glass areas at each facade. All awnings are to be metal or glass (no fabric).
 - K. Residential Density and Mixture:
 1. See Section 12C-1063.08 for Residential Density Table.
 2. Residential Density shall include a blend of compatible shapes, colors, and sizes of homes within the development plan. The variety of housing types will provide NLC citizens with options and establish neighborhoods with various architecture styles and Building Types.
 3. Adjacent homes in the SF6 zone must vary on the following characteristics: (Roofline, floor plan layout, elevation). Mirror images of adjacent homes are not allowed.
 4. The construction of Garages are regulated in each Zone as shown in Section 12C-1066.
 - L. Streets and Blocks. Streets and blocks shall conform to Section 12C-1064.

Amended by Ord. 13-02 on 5/1/2013

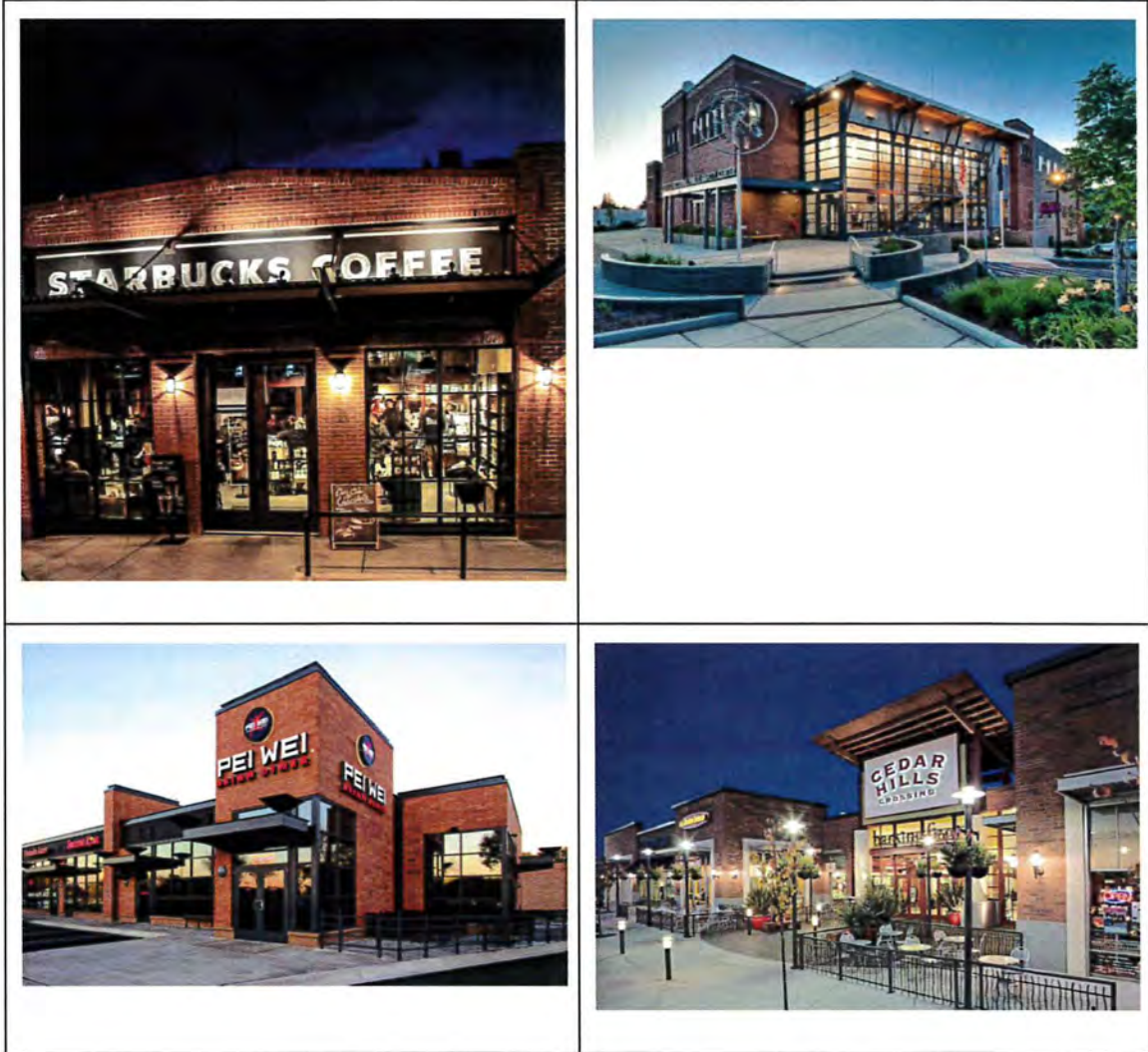
Amended by Ord. 16-05 on 9/7/2016

12C-1063.13 City Center Commercial (CCC) - Specific Requirements

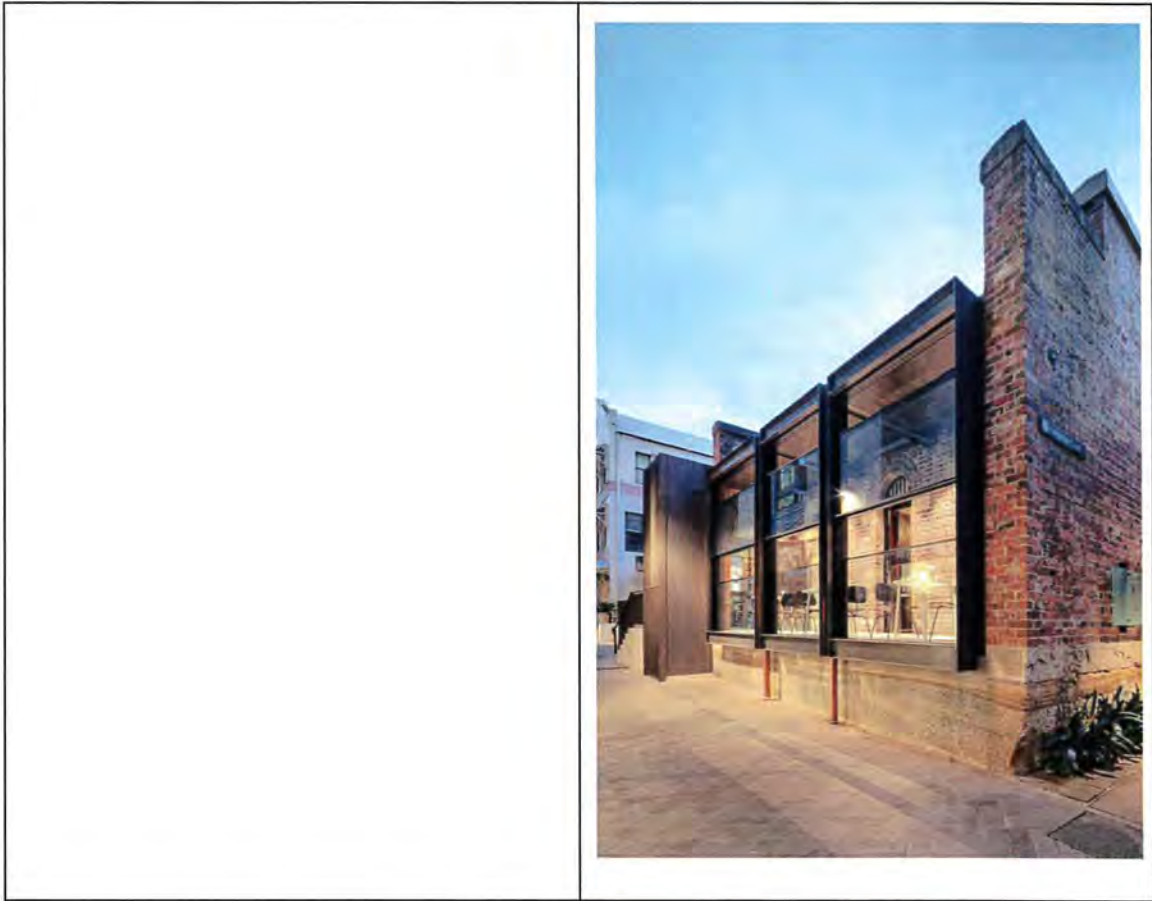
- A. City Center Commercial (CCC) Zone is a transition Zone between the City Center and Highway 91 commercial. The CCC will primarily be commercial or office buildings with slightly wider lots than CCA or DT and with a wider variety of setbacks. The CCC Zone requires parking to be on the side or rear of the lots and with buildings on the street to encourage walkability.

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B. Examples: CCC



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- C. All plans for proposed projects must conform to the City Center Code 12C-1060 through 12C-1069 and all other applicable North Logan City ordinances.
- D. For the City Center Commercial (CCC) Zone shown on the Regulating Plan, Special Requirements and standards shall be applied as follows:
- E. Building Frontage. The CCC is a Mixed-Use Zone and commercial buildings are permitted. Commercial retail buildings have the following requirements in this zone:
 - 1. The street facade of the buildings should provide a Shop Front appearance at the sidewalk level.
 - 2. The building front shall consist of all building facades facing the primal street.
 - 3. The vertical height on the Shop Front shall be from ground level up to a minimum of 10 feet vertical height.
 - 4. The building front shall be no less than 50% glazed in clear glass, on at least the first floor.
- F. Density.
 - 1. The CCC Zone requires commercial uses at the main floor level, but residential is permitted on the second and third floors. All residential units above the main floor require an access separate from the entrance to the commercial use on the 1st floor of the building.
 - 2. The CCC Zone does not require residential units as part of the Development Plan.
 - 3. See Section 12C-1063.08 for Density Table.

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- 4. All residential units must meet the off-street Parking requirements per building requirements as shown in the Building Forms Requirements Section 12C-1066 and Open Space requirements as shown in 12C-1065.
- G. Streets and Blocks. Streets and blocks will conform to Section 12C-1064.

H. Building Materials:

1. Buildings in the CCC zone must have high quality exterior finish materials on all sides. High quality exterior finish materials are defined as brick, fiber-cement board or similar lap siding, glass, steel, or stone. Brick is to be used on 50% (minimum) of all non-glass areas on each facade. All awnings are to be metal (no fabric).

Adopted by Ord. 12-09 on 12/19/2012

12C-1063.14 Hospital Zone (HOSP) City-Center - Specific Requirements

Development within that portion of the HOSP Zone between 200 and 400 East and between 1800 and 2000 North shall comply with the following requirements in addition to the requirements found elsewhere in Title 12C-Zoning for the HOSP Zone:

- A. Building Types shall follow the pattern of the City Center Code,
- B. A development plan of the entire project, building plans according to all applicable codes, a transportation plan including streets, walks, and trails, parking plans, open space plan, landscaping plans, lighting plans, and updated use standards shall be provided for the development within this HOSP Zone, and
- C. The hospital buildings and development plan shall conform to the Complete Streets Section 12C-1064, the North Logan City Transportation Plan, and as much as possible to the intent of the City Center as outlined in Sections 12C-1060 through 1069.
- D. 1. Buildings in the HOSP zone must have high quality exterior finish materials on all sides. High quality exterior finish materials are defined as brick, fiber-cement board or similar lap siding, glass, steel, or stone. Brick is to be used on 50% (minimum) of all non-glass areas on each facade. All awnings are to be metal (no fabric).

Adopted by Ord. 12-09 on 12/19/2012

12C-1064 Complete Streets

12C-1064.01 Street Classifications

12C-1064.02 Comparable Functional Classification

12C-1064.03 Street Map For The City Center

12C-1064.04 General Street Requirements

12C-1064.05 General Alley Standards

12C-1064.06 Block Standards

12C-1064.07 200 East (the North Logan - Hyde Park Transportation Corridor)

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12C-1064.08 Street Trees

12C-1064.09 80' Collector/Arterial Street

12C-1064.10 80' 2200 North Street (See Streets Map 12C-1064.03)

12C-1064.11 Downtown Street

12C-1064.12 Commercial Street

12C-1064.13 Residential Street

12C-1064.14 Alley

12C-1064.01 Street Classifications

This section identifies the various street types used to complete the network of the Complete Streets Plan. Street Classifications shall be applied to all new platted streets in the City Center (with the exception 200 East which shall be designed in compliance with the Record of Decision for the North Logan – Hyde Park Transportation Corridor EIS). City sponsored public improvements should also meet these standards. These standards supersede any conflicting standards or policies of the City ordinances.

Adopted by Ord. 12-09 on 12/19/2012

12C-1064.02 Comparable Functional Classification

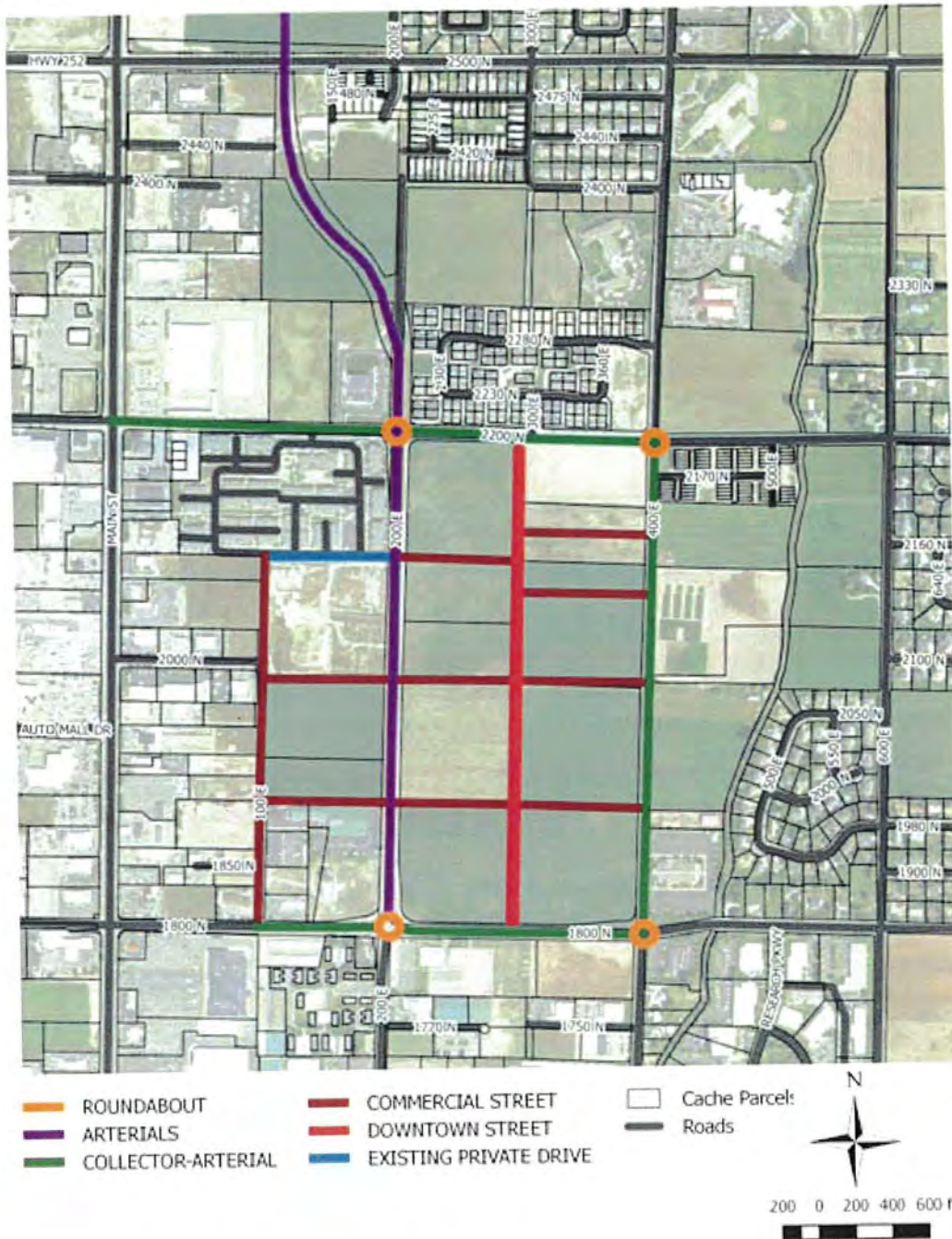
The street classification references to the functional classification system are derived from the City Center Plan. They are meant to be used as a guide to describe their basic characteristics. Detailed design standards must reflect those in this section. The function of all street types is to promote walkability, pedestrian and bicyclist safety with vehicle mobility as a companion to pedestrian travel, biking and outdoor living.

Adopted by Ord. 12-09 on 12/19/2012

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12C-1064.03 Street Map For The City Center



Adopted by Ord. 12-09 on 12/19/2012

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Amended by Ord. 16-06 on 9/7/2016

12C-1064.04 General Street Requirements

- A. Streets must extend to undeveloped parcels and connect with existing roads. Stubs must be provided to adjoining undeveloped areas to accommodate future connectivity
- B. The street network should respect the topography and any identified environmental resources on the site
- C. Dead-end streets or cul-de-sac streets are not permitted except where physical conditions such as highways, sensitive natural resources, waterways, canals or unusual topography provide no practical connection alternative.
- D. All new public streets must accommodate on-street parking, with the exception of 200 East.
- E. Any central turn lane may be approved by the City only after a traffic evaluation.
- F. All streets must be designated for public use. ~~Private streets and closed or gated streets are prohibited.~~ The City Center street cross sections should be designed to accommodate pedestrian and bike usage, along with parking requirements for the general public. However, the maintenance, snow removal and liability for the sidewalks, park strips, street trees shall lie with the adjacent real property owner or the associated HOA.
- G. All street must meet North Logan City engineering technical design standards (NLC-DSTM) and AASHTO requirements.
- H. All planned and designed streets in the City Center should match the street type requirements from Section 12C-1064 and the North Logan City Transportation Plan. Future streets in the City Center not shown on North Logan City Transportation Plan will be assigned a street type from this code section (12C-1064) by the City.
- I. Street designs should match right-of-way widths of adjoining developments to be compatible and approved by the City.
- J. All streets shall have curb, gutter, swales, and sidewalk according to North Logan City standards.
- K. All development within the Downtown Zone shall further comply with specific architectural styles for lighting fixtures, sidewalk paving, intersection paving, benches, landscape planters, landscaping, tree grates, and other site furnishings. The Developer shall comply with the City Center Design Guidelines as directed by the City and the Design Review Committee.

Adopted by Ord. 12-09 on 12/19/2012

Amended by Ord. 16-06 on 9/7/2016

12C-1064.05 General Alley Standards

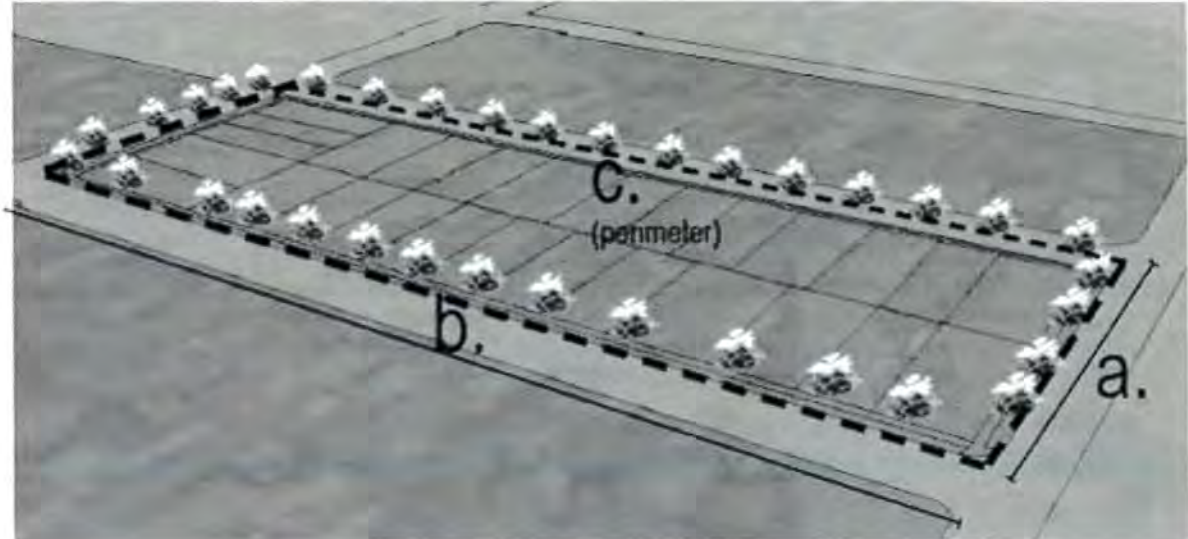
- A. Where Alleys can be accommodated, a continuous network of Alleys **must** be planned to connect the service side of commercial and or condominium/apartment buildings. Rear and side alleys must serve as the primary means of vehicular ingress to individual lots. **Alleys must be privately owned and maintained. Any cross section that is less than 50' in width shall be privately owned and maintained.**
- B. A continuously connected rear or side circulation aisle within a parking area may provide an acceptable alley alternative where exceptional circumstances or existing conditions are present. Cross access easements between parking areas shall be provided and coordinated with neighboring landowners and the City.

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C. All Alleys shall be designated open for public use and the Development Agreement shall indicate such.

D.

E. **BLOCK DIMENSIONS DIAGRAM**



F.

Adopted by Ord. 12-09 on 12/19/2012

Amended by Ord. 16-06 on 9/7/2016

12C-1064.06 Block Standards

- A. Smaller blocks are encouraged to provide enhanced pedestrian connectivity.
- B. Blocks that are rectangular or rectilinear shall have the following standards:
 - 1. Minimum length of anyone side is 150 feet
 - 2. Maximum length of anyone side is 660 feet
 - 3. Maximum perimeter of any block measured at the Right-Of-Way (ROW) line or natural boundary is 2,640 feet
- C. Blocks that are trapezoidal or irregular:
 - 1. Minimum length of anyone side is 100 feet
 - 2. Maximum average of the two longest sides is 660 feet.
 - 3. Maximum perimeter of any block measured at the ROW line or natural boundary is 2,640 feet
- D. The 660-ft. by 660-ft. approximate block/grid was established by pioneers and has proven to be a successful block system. For residential neighborhoods in the City Center developers should use smaller blocks (380' x 680') or smaller.

Adopted by Ord. 12-09 on 12/19/2012

12C-1064.07 200 East (the North Logan - Hyde Park Transportation Corridor)

200 East has been planned for and classified as a major arterial street for North Logan City. From 1800 North to 2500 North the street 200 East shall be a limited access street. Access should be limited to cross-street intersection points.

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Adopted by Ord. 12-09 on 12/19/2012

12C-1064.08 Street Trees

- A. All street trees shall be of a large deciduous species.
- B. Street trees shall be of a single species per Block Face on both sides. Street tree species and arrangements shall be planned according to the City Center street tree approved species list and guidelines.
- C. (D) Street tree spacing shall be according to street standards in 12C-1064.

Adopted by Ord. 12-09 on 12/19/2012

12C-1064.09 80' Collector/Arterial Street

- A. A two-lane Thoroughfare road leading to the City Center and Neighborhoods.
- B. Collector/Arterial; Street Design Requirements:

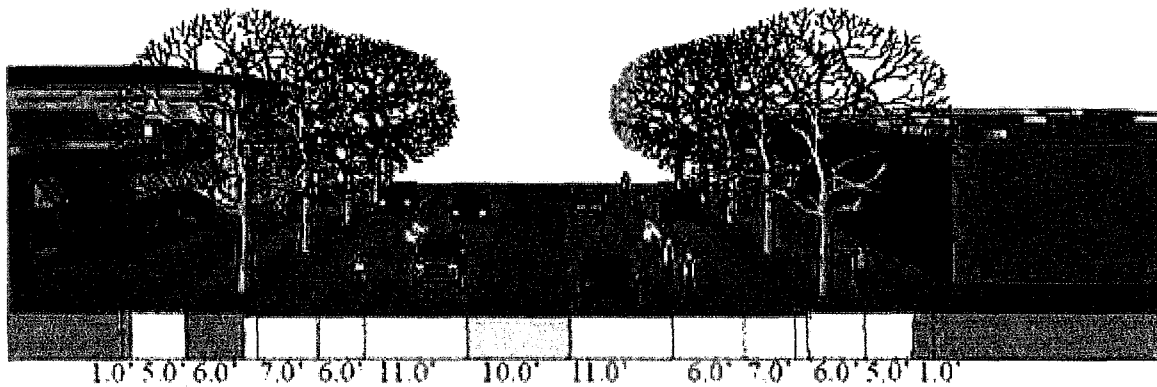
Vehicle Movement:	Free
Street Edge:	Vertical, square curb
Park Strip:	Plantings or Grass
Pedestrian Bump-outs:	Not Required
On-street Parking:	Parallel
Turn Lane:	Intersections Only
Bike Lane:	Required
Center Boulevard Planter:	Optional

	Min.	Max.
Corner Radius:	5'	30'

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Tree Spacing:	25'	35'
Park Strip (or swale where applicable):	5'	8'
Sidewalk or Trail Width:	5'	8'
Parking Width:	7'	8'
Drive Lane Width:	11'	12'
Turn Lane Width: (If / Where Approved)	10'	11'
Center Boulevard Planter: (If / Where Approved)	6'	10'
Bike Lane:	5'	7'
One Foot Offset for Property Line	2'	2'
Total Right-of-Way:	68'	99'

C. EXAMPLE RIGHT-OF-WAY 82 FEET



D.

Adopted by Ord. 12-09 on 12/19/2012

Amended by Ord. 16-06 on 9/7/2016

12C-1064.10 80' 2200 North Street (See Streets Map 12C-1064.03)

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- A. A two-lane Thoroughfare road leading to the City Center and Neighborhoods.
- B. 2200 North Street Design Requirements:
- C.

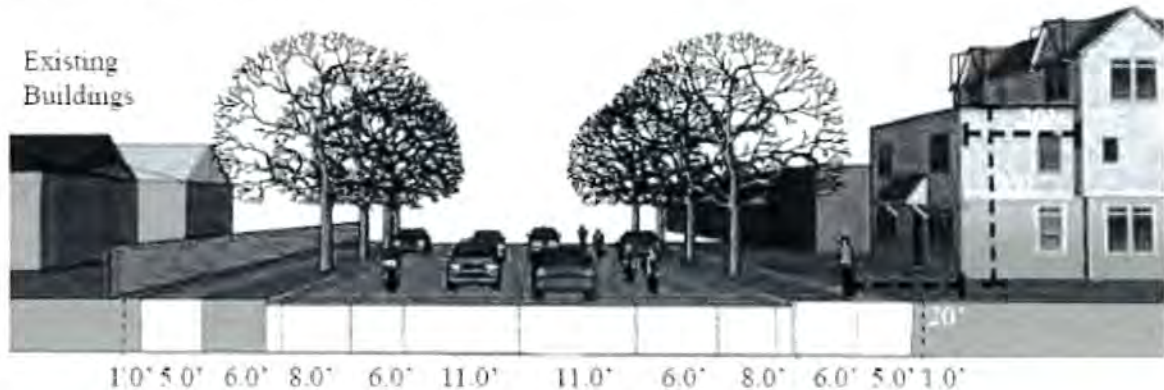
Vehicle Movement:	Free
Street Edge:	Vertical, square curb
Park Strip:	Plantings or Grass
Pedestrian Bump-outs:	Not Required
On-street Parking:	Parallel
Turn Lane:	Intersections Only
Bike Lane:	Required
Center Boulevard Planter	Optional

	Min.	Max.
Corner Radius:	5'	30'
Tree Spacing:	25'	35'
Park Strip (or swale where applicable):	5'	8'
Sidewalk or Trail Width:	5'	8'
Parking Width:	7'	8'

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Drive Lane Width:	11'	12'
Turn Lane Width: (If / Where Approved)	10'	11'
Center Boulevard Planter: (If / Where Approved)	6'	10'
Bike Lane:	5'	7'
One Foot Offset for Property Line	2'	2'
Total Right-of-Way:	66'	97'

D. EXAMPLE RIGHT-OF-WAY 74 FEET



E.

F. On 2200 North between 200 East and 400 East the building setback is twenty feet (20') and the vertical building height limit on the facade of the building at the setback is limited to twenty-two feet (22'). Behind the front twenty-foot (20') depth of the façade, the height of the building may extend to a standard the roofline height (see illustration above).

Adopted by Ord. 12-09 on 12/19/2012

Amended by Ord. 16-06 on 9/7/2016

12C-1064.11 Downtown Street

- A. The most urban street type, the Downtown Street incorporates the highest quality of materials and design to create a unique character with the adjacent development. Dedicated on-street parking, wide sidewalks, and urban style street tree planters are typical. The Downtown will look and function as a historic main street. Mid-block crossings will be required on block distances greater than 330 feet per single side.
- B. Downtown Street Requirements:

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C.

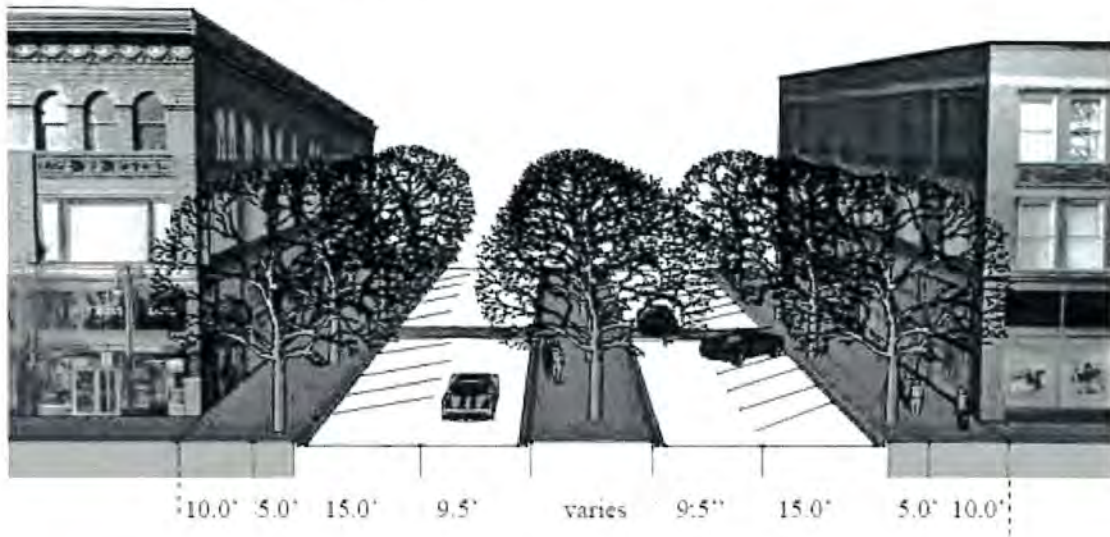
Vehicle Movement:	Slow
Street Edge:	Vertical, square curb
Park Strip (or swale where applicable):	Wells or Grates or Planters
Pedestrian Bump-outs:	Intersections & Crosswalks
On-street Parking:	45 Degree
Turn Lane:	Central
Bike Lane:	Not Required
Center Boulevard Planter:	Required

	Min.	Max.
Corner Radius:	5'	25'
Tree Spacing:	25"	30'
Park Strip:	5'	8'
Sidewalk or Trail Width:	10'	14'
Parking Width (45°)	14'	16'
Drive Lane Width:	10'	11'

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Turn Lane Width: (If / Where Approved)	10'	10'
Center Boulevard Planter: (If / Where Approved)	7'	10'
Bike Lane: (If / Where Approved)	5'	7'
Total Right-of-Way:	85'	108'

D. EXAMPLE RIGHT-OF-WAY 89 FEET



E.

Adopted by Ord. 12-09 on 12/19/2012

12C-1064.12 Commercial Street

- A. This street provides access to the City Center and neighboring Zones.
- B. Commercial Street Design Requirements:
- C.

Vehicle Movement:	Slow
Street Edge:	Vertical, Square curb
Park Strip (or swale where applicable):	Plantings or Grass
Pedestrian Bump-outs:	Recommended

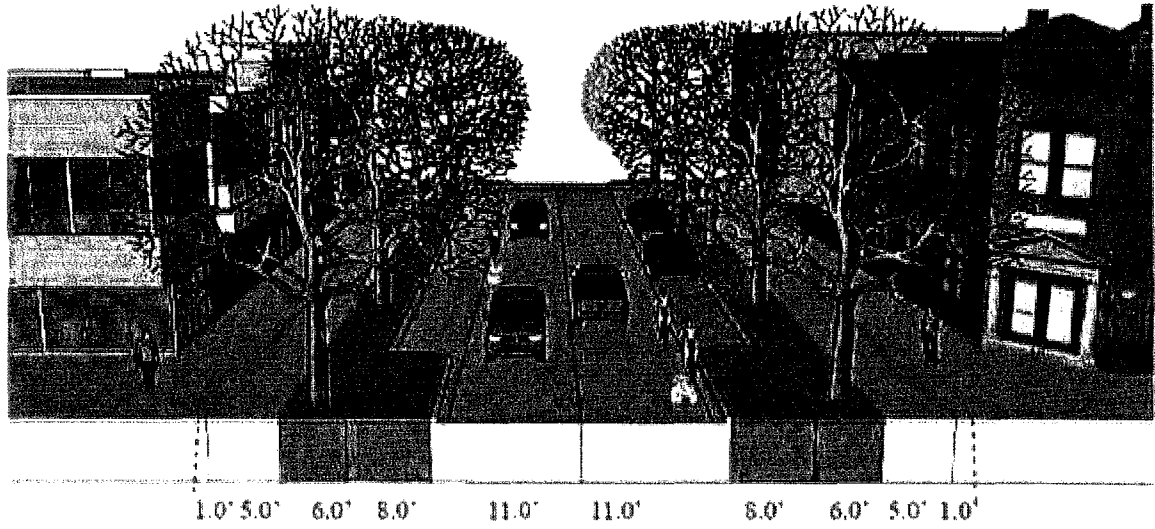
ORDINANCE NO. 17 – 09
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On-street Parking:	Parallel
Turn Lane:	Intersections Only
Bike Lane:	Shared Lane
Center Boulevard Planter:	None

	Min.	Max.
Corner Radius:	5'	25'
Tree Spacing:	25'	30'
Park Strip:	5'	8'
Sidewalk or Trail Width:	7'	12'
Parking Width:	8'	9'
Drive Lane Width:	11'	11'
Turn Lane Width: (If / Where Approved)	10'	11'
Bike Lane: (Share Lane with Vehicles)	N/A	N/A
One Foot Offset for Property Line	2'	2'
Total Right-of-Way:	60'	91'

D. EXAMPLE RIGHT-OF-WAY 62 FEET

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E.

Adopted by Ord. 12-09 on 12/19/2012

Amended by Ord. 16-06 on 9/7/2016

12C-1064.13 Residential Street

A. A typical two travel lane local street. This street is not heavily traveled but is important to provide connectivity and disperse traffic. On-street yield parking, park strips, and sidewalks make this an intimate street. Bicycle Shared Lane Symbols will be required per block and bike/pedestrian plan. Developers shall use the Residential Street Requirements for streets in the MR7 and MR8 Zones unless approved otherwise by the City.

B. Residential Street Design Requirements:

C.

Vehicle Movement:	Yield
Street Edge:	Curb or Concrete Band
Park Strip (or swale where applicable):	Plantings or Grass
Pedestrian Bump-outs:	Recommended
On-street Parking:	Yield
Turn Lane:	Intersections Only

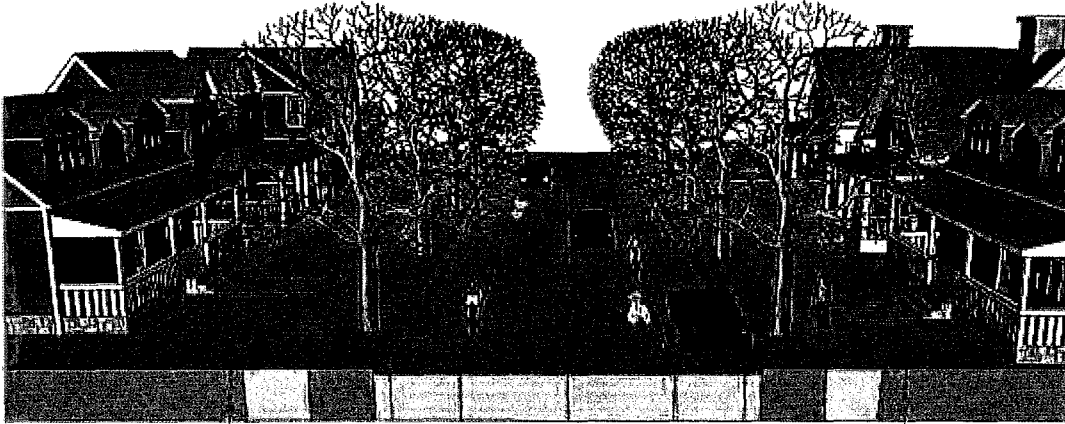
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Bike Lane:	Shared Lane
Center Boulevard Planter:	None

	Min.	Max.
Corner Radius:	5'	15'
Tree Spacing:	25'	30'
Park Strip:	5'	8'
Sidewalk or Trail Width:	5'	6'
Parking Width:	8'	9'
Drive Lane Width:	10'	11'
Turn Lane Width: (If / Where Approved)	10'	11'
Bike Lane: (Share Lane with Vehicles)	N/A	N/A
One Foot Offset for Property Line	2'	2'
Total Right-of-Way:	58'	68'

D. EXAMPLE RIGHT-OF-WAY 62 FEET

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1.0' 5.0' 6.0' 8.0' 11.0' 11.0' 8.0' 6.0' 5.0' 1.0'

E.

Adopted by Ord. 12-09 on 12/19/2012

12C-1064.14 Alley

- A. A service road to the rear of development linking Parking fields and minimizing the need for curb cuts along adjacent roadways. Two-way operation is important but not required.
- B. Alley Design Standards:
- C.

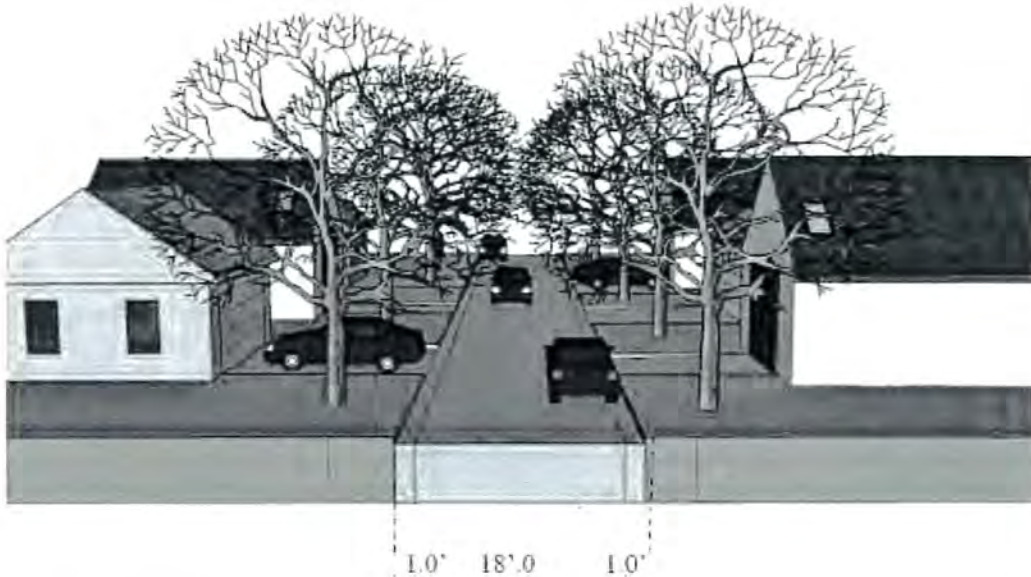
Vehicle Movement:	Yield
Street Edge:	Curb or Concrete Band
Park Strip (or swale where applicable):	Plantings or Grass
Pedestrian Bump-outs:	None
On-street Parking:	None
Turn Lane:	None
Bike Lane:	None
Center Boulevard Planter:	None

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	Min.	Max
Corner Radius:	5'	15'
Tree Spacing:	no min	no max
Landscape Area:	4'	no max
Sidewalk Width:	none	none
Parking Width:	none	none
Drive Lane Width:	12' if One Way'	20' if Two Way'
Turn Lane Width: (If / Where Approved)	none	none
Bike Lane:	none	none
Total Right-of-Way:	14'	20'

D. EXAMPLE RIGHT-OF-WAY 20 FEET

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E.

Adopted by Ord. 12-09 on 12/19/2012

12C-1065 Open Space Requirements

12C-1065.01 Minimum Required Open Space Requirements (Note - There Is No Maximum):

12C-1065.02 City Center Open Space, Bike Lanes And Trails Map

12C-1065.03 Open Space

Open Space is to help create healthy communities and happy citizens. In general Open Space includes a variety of landscape types such as improvements, Common Open Space, Common Greens, trails, civic greens, community gardens, Town Square, nature Parks and other open areas.

Examples:

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Adopted by Ord. 12-09 on 12/19/2012

12C-1065.01 Minimum Required Open Space Requirements (Note - There Is No Maximum):

Percentage of gross land as open space	Minimum
Downtown (DT)	10% (Optional 5% reduction* in lieu of contribution)
City Center Adjacent (CCA)	20% (Optional 5% reduction* in lieu of contribution)
City Center Commercial (CCC)	10% (Optional 5% reduction* in lieu of contribution)
Mixed Residential 8 (MR8)	15%
Mixed Residential 7 (MR7)	15%
City Center Residential Single Family 6 (SF6)	0%

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* In lieu of 5% Open Space - Developer shall contribute 5% land to actual Town Square or financial donation to the North Logan City – Town Square construction. Contribution must be equivalent or greater than appraised market value of land and landscaping improvements of open space square footage in the City Center area (actual parcel). The "In Lieu of Open Space Contract" shall be approved via Development Agreement with the City.

Adopted by Ord. 12-09 on 12/19/2012

Amended by Ord. 16-05 on 9/7/2016

Amended by Ord. 16-06 on 9/7/2016

12C-1065.02 City Center Open Space, Bike Lanes And Trails Map



Adopted by Ord. 12-09 on 12/19/2012

12C-1065.03 Open Space

- A. Open space is an area of land or water that may be used for passive or active recreation, conservation, landscaped areas, preserves of the natural environment, scenic land, and/or other use that is of a suitable size, topography, location, and shape to permit the activities for which it is intended as determined by the City. The required Open Space percentage shall be per lot/property and shall not include land within the City's Right-of-Way (ROW).
- B. The following qualify as meeting the Open Space Requirements in a Development Plan: plazas, courtyards, Common Green, Common Open Space, playing fields, gardens, shade structures, fountains, sidewalks, trails, play areas, sports courts, and landscaping.
- C. The following do not qualify to meet Open Space Requirements: buildings, sheds, garages, drives, roads, streets, Parking area, service docks or utility areas. These are not comprehensive lists. Any qualifying Open Space shall be considered by the City for approval in order to qualify as such.
- D. Land and or landscaping on Lots with the following Building Types shall not be credited towards the Open Space Requirement: Small Single-Family, Medium Single-Family, Two-Family Home, and Townhome (backyard only). Townhome front yards may count towards the Open Space Requirement.
- E. Storm water areas shall only count towards Open Space when they are primarily designed to serve all of the Open Space requirements and intents. For example a big grass play field, which doubles as a stormwater basin would qualify. Storm water areas may only account for up to 30% of the total Open Space requirements.
- F. In the MR7, MR8 and CCA Zones, a minimum of one (1) Common Green shall be required when the Development Plan has more than 20 Dwelling Units.
- G. Common Greens in residential developments or residential portions of developments shall include at least the following components: playground for children, grass open space for play and recreation, benches, landscaping per section 12C- 1067, and shall be considered for conceptual and final approval by the City per the Development Plan and Agreement.

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- H. The Developer shall provide an Open Space maintenance plan as part of the Development Agreement and include details regarding landscape maintenance, recreation equipment maintenance and so forth.
- I. Trails are an essential component to the City Center plan and the City's recreational plan. Developers shall review the City's Parks and Recreation Plan and implement required trails according to the City's Plan. All trails shown on 12C-1065.02 shall be a minimum of 8' wide and final location and construction per the Development Plan and Agreement.
- J. Developers shall be required to implement and build the approved Open Space improvements according to the Development Agreement with the City before building occupancy permits are issued and or improvement bonds released.
- K. All open space shall be privately owned and maintained, unless it is donated as part of the town square/village green area.

Adopted by Ord. 12-09 on 12/19/2012

12C-1066 Building Form Requirements

- 12C-1066.01 Building Orientation Section
- 12C-1066.02 Small - Single Family (SSF)
- 12C-1066.03 Medium - Single Family (MSF)
- 12C-1066.04 Two - Family Home (TFH)
- 12C-1066.05 Townhome (TH)
- 12C-1066.06 Multi-Unit House (MUH)
- 12C-1066.06 Multi-Unit Building (MUB)
- 12C-1066.07 Classic Commercial Building (CC)
- 12C-1066.08 City Commercial Building (CCB)
- 12C-1066.09 Garages
- 12C-1066.10 Parking Structures (PS)

- A. Building Form Introduction. Ten (10) Building Types are identified for the City Center. A collection of Building Types with similar form and scale define the environment of each Zone in the City Center.
 - 1. Building Types are permitted in corresponding Zones. Where a grid unit is marked with an "X", the building type will be permitted in that Zone.
 - 2. Lot standards also correspond to each building type. These standards may vary depending on the Zone in which the Building Type is permitted.

NOTE: THIS TABLE DID NOT TRANSPOSE CORRECTLY. SHOULD REFLECT WHAT IS IN THE EXISTING CODE ONLINE WITH THE (MUH) REMOVED. THIS WILL BE THE CASE WITH ALL THE TABLES THAT DID NOT TRANSPOSE CORRECTLY INTO THIS GOOGLE FORMAT.

3. PERMITTED BUILDING TYPES					
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9. Small-Single Family (SSF)					
15. Medium-Single Family (MSF)					
21. Two-Family Home (TFH)					
27. Townhome (TH)					
33. Multi-Unit House (MUH)					
39. Multi-Unit Building (MUB)					
45. Classic Commercial (CC)					
51. City Commercial (CCB)					
57. Parking Structures (PS)					
63. Residential Garages (G)					

- B. The City Center Code and the Building Types in these Zones do not prescribe any specific architectural style. Massing diagrams and benchmark images used represent the ~~desired~~ **required** form, massing, scale, and qualities for development in the City Center. It is inherent that certain architectural styles represent slight variations to the diagrams and benchmark images provided. These types of variations are permitted as long as they meet the requirements of the City Center Design Criteria, Commercial Design Guidelines and Design Review Committee requests.
- C. All requirements of the City Center Design Criteria, Commercial Design Guidelines, North Logan City Codes Requirements, and IBC Codes are required according to City building permits.
- D. Any building existing prior to the passage of this City Center Ordinance which is nonconforming due to the fact that it is not a permitted building type may be enlarged, extended,

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- reconstructed or structurally altered only if such modifications meet the requirements of the City Center Design Criteria, Commercial Design Guidelines and all other development standards.
- E. Parking standards also correspond to each building type. In the event Parking spaces in a development meet the City Center Code at the time of development, and if the use of the building(s) changes, the following shall regulate such a change:
 - 1. If the new parking requirement is less, there shall be no requirement to remove Parking spaces to meet maximum Parking space requirements.
 - 2. If the new required parking is greater than that found in the development, additional parking to meet the minimum Parking spaces must be provided to meet any new requirement.
 - F. Additional/alternate Building Types not represented in this code can be considered for approval by the Design Review Committee and the City.
 - G.
 - H. For an alternate Building Type to be considered, the applicant must prepare graphic exhibits and lot standards that correspond to Permitted Building Types NLC Center Code 12CI060-12CI070 and the desired placement in a Zone and show how the alternate Building Type meets the intent of this City Center Code.
 - I. Alternate Building Types that are not in section 12C-1066 and approved by the City shall be project specific and cannot be used for other development applications.
 - J. In all City Center Zones, regardless of the building size, all building facades facing a public or private street shall vary from all other lots on a single Block Face. Each facade shall vary in a minimum of three of the following methods: architecture style, color, building materials, roofline, window treatments, or architecture details.
 - K. In the DT Zone, the width of each unique facade shall not be more than eighty-five (85') feet in length on the building front facing the street.
 - L. In the DT and CCC Zones, no building next to each other shall have the same roof line elevation and articulation.

Roofline variation from building to building example.



- M.
- N. Developers and builders **must** use Timeless materials such as brick, stone, and others approved by the City and the Design Review Committee. The development **must** be consistent with the City Center Design Criteria document.
- O. In the City Center and adjacent Zones the vertical setback or transition from one-story residential dwelling to three stories building shall use one of the following options.

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1. Option A - In between a one-story residential dwelling and a three-story building there shall be a two-story building.



2.
 3. Option B - when the one-story residential dwelling is directly across the street or diagonal from the proposed three-story building the three-story building shall be two stories with a maximum height of twenty-two (22) feet and horizontal and a three-story vertical setback of 20 feet before going to full height (see the diagram below).



4.
 P. Buildings with lot widths that are longer than 200 feet should be sensitive to adjacent building patterns. If a wide building is across from an existing set of buildings with significantly narrow lot widths, the wide building must mitigate the facade length by creating the appearance of smaller lot widths. Wide buildings may implement vertical architectural elements to the building facade, changes in material, color, breaks in plane to the facade, or a varied roof line.
 Q. Residential Buildings (Single-family Homes, Two-family Homes, Townhomes, Multi-unit homes and Multi-unit Building) shall be oriented according to one of the methods shown below (Traditional, Connected Courtyard, or Courtyard see 12C-1066.01). The diagrams below represent general site design principles and should be followed as much as possible according to the project specific architecture, site layout and terrain.

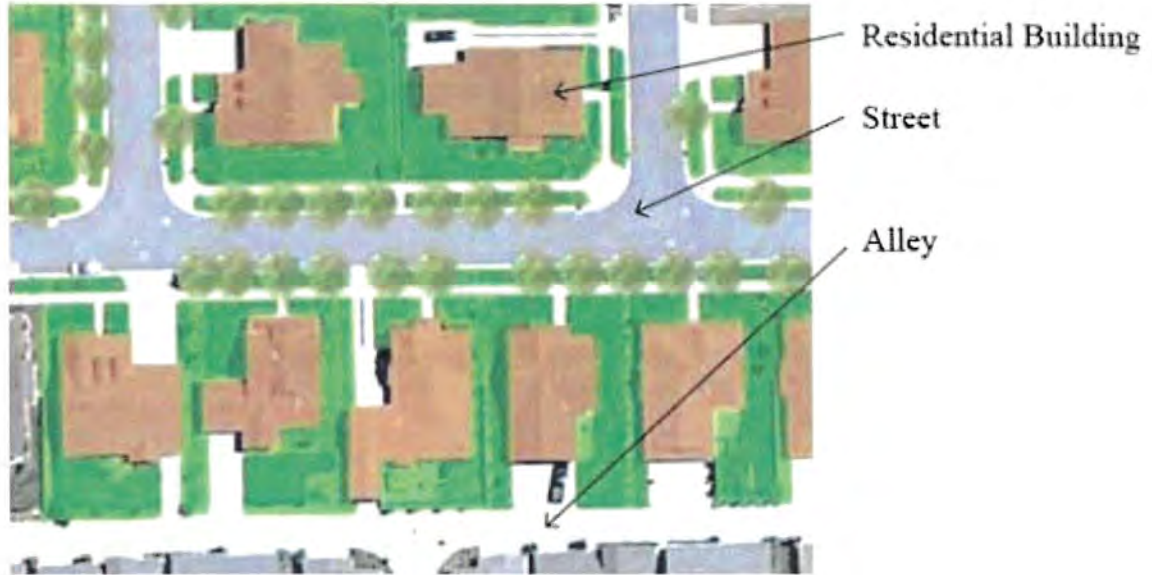
Amended by Ord. 16-05 on 9/7/2016

Amended by Ord. 16-06 on 9/7/2016

12C-1066.01 Building Orientation Section

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- A. All residential buildings shall be orientated in one of the following methods:
- B. Traditional Street Orientation (for SSF, MSF, TF, TFH, ~~MUH~~, MUB, SF6 and TH).

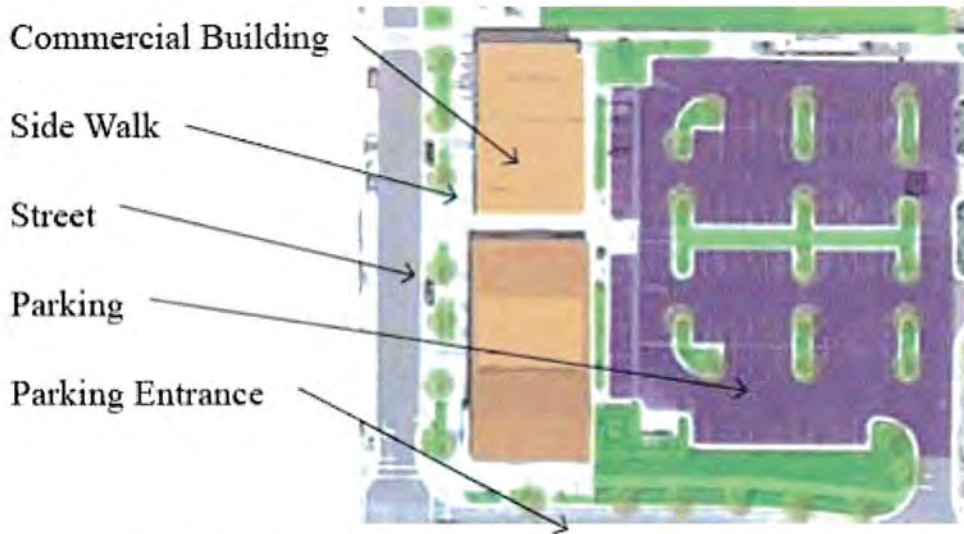


- C.
- D. Connecting Common Space (for SSF, MSF, TF, TFH, MUH, MUB, SF6 and TH).
- E. Common Space (for SSF, MSF, ~~TF, TFH, MUH~~, MUB, SF6 and TH).



- F.
- G. Commercial Buildings (Classic Commercial and City Commercial Building) shall be oriented according to one of the methods shown below (Rear Parking, Side Parking or Corner Layout).
- H. Parking options depend according to building type and building width requirement per Zone. The diagrams below represent general site design principles and should be followed best according to the project specific architecture, site design and terrain.
- I.
- J. Commercial Building and site design are as follows:
- K.
- L. Commercial Building Rear Parking (for CC, MUB and CCB buildings)

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- M.
- N. Commercial Corner Building (for CC, MUB and CCB buildings)

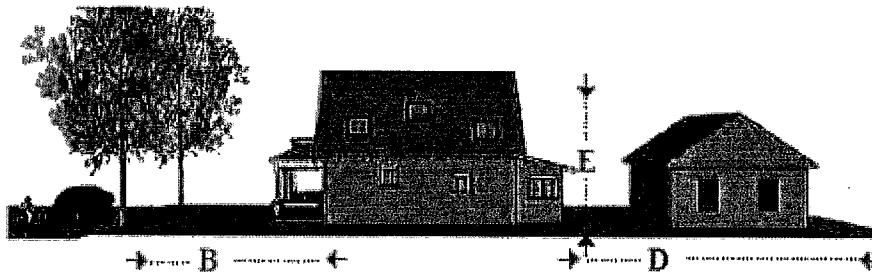
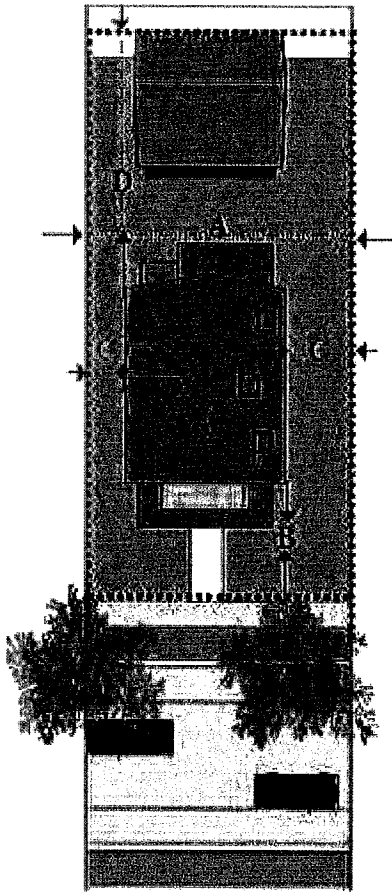
- O. Commercial Building/Side Parking Lot (for CC, MUB and CCB buildings)

Adopted by Ord. 12-09 on 12/19/2012

12C-1066.02 Small - Single Family (SSF)

- A. A small size detached home residence that is typically 1-2 stories.
- B. Typical Lot Configuration and Benchmark Examples:

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Permitted Zones

DT	CCC	CCA	MR8	MR7
no	no	no	yes	yes

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Examples: SF6



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- C. Lot & Building Requirements - Small-Single Family.
- D. The following building and lot standards shall be utilized for Small-Single Family Homes.
- E.

	DT	CCC	CCA	MR10 8	MR7	SF6
	Not Permitted	Not Permitted	Not Permitted	Min.	Max.	Min.
Lot Area:				4,500 sf	6,500 sf	
Lot Coverage:				no min.	60%	No min.

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Lot Width (A):				45'	65'	50'
Front Setback (B):				7'	20'	20'
Side Setback (C):				7'	no max.	5'
Rear Setback - Home (D):				15'	no max.	15'
Building Width:				60%	90%	
Stories:				1	2	2
Height (E):				11'	25'	11'

1. Lot Access Standards
 - a. Where an alley is present, Parking and services shall be accessed through the alley.
 - b. Where an alley is not present, Parking and services shall be accessed by a driveway of 8' to 18' wide, with at least a 3' landscaped side yard setback.
 - c. On a corner lot without access to an alley, Parking and services shall be accessed by a driveway off of the street not faced by the primary entrance to the building. These driveways may be up to 18' wide at the top of the apron with a 3' landscaped side or rear yard setback.
2. Parking Standards:
 - a. Minimum of two (2) off-street space per Small Single Family.
 - b. An alley accessed lot must provide Parking in the rear yard. A non-alley accessed lot shall provide spaces in either the side yard behind the building frontage or in the rear yard.
 - c. Parking in the Park strip is prohibited.
 - d. Garages (covered Parking) shall be in accordance to Section 12C-1066.10.
3. Service & Utility Standards: Above ground mechanical devices shall be located in the side or rear yard, behind all portions of the principal facade, and shall be fully screened from the street and neighboring properties.
4. Building Frontage Standards:
 - a. At least one (1) functioning entrance to the building shall be provided from the street.
 - b. SSF on corner lots shall be designed to include windows and at least one vertical plane break in elevation on the second side facing the street or courtyard.

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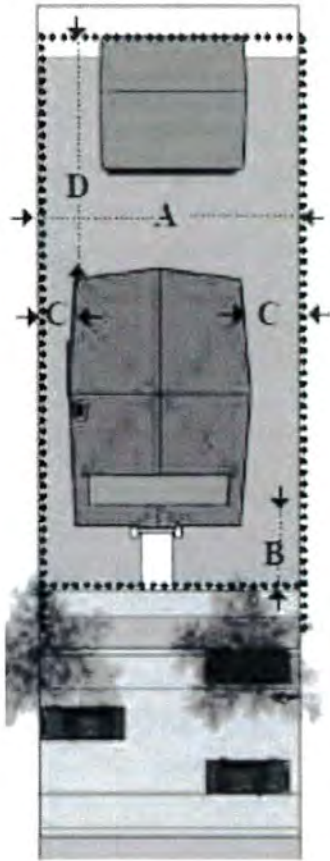
- c. SSF homes should have a porch on the street front side of the building.
- 5. Landscaping shall be required per Section 12C-1067.

Adopted by Ord. 12-09 on 12/19/2012

12C-1066.03 Medium - Single Family (MSF)

- A. A medium sized detached home residence that is typically 1-2 stories.
- B. Typical Lot Configuration and benchmark examples:

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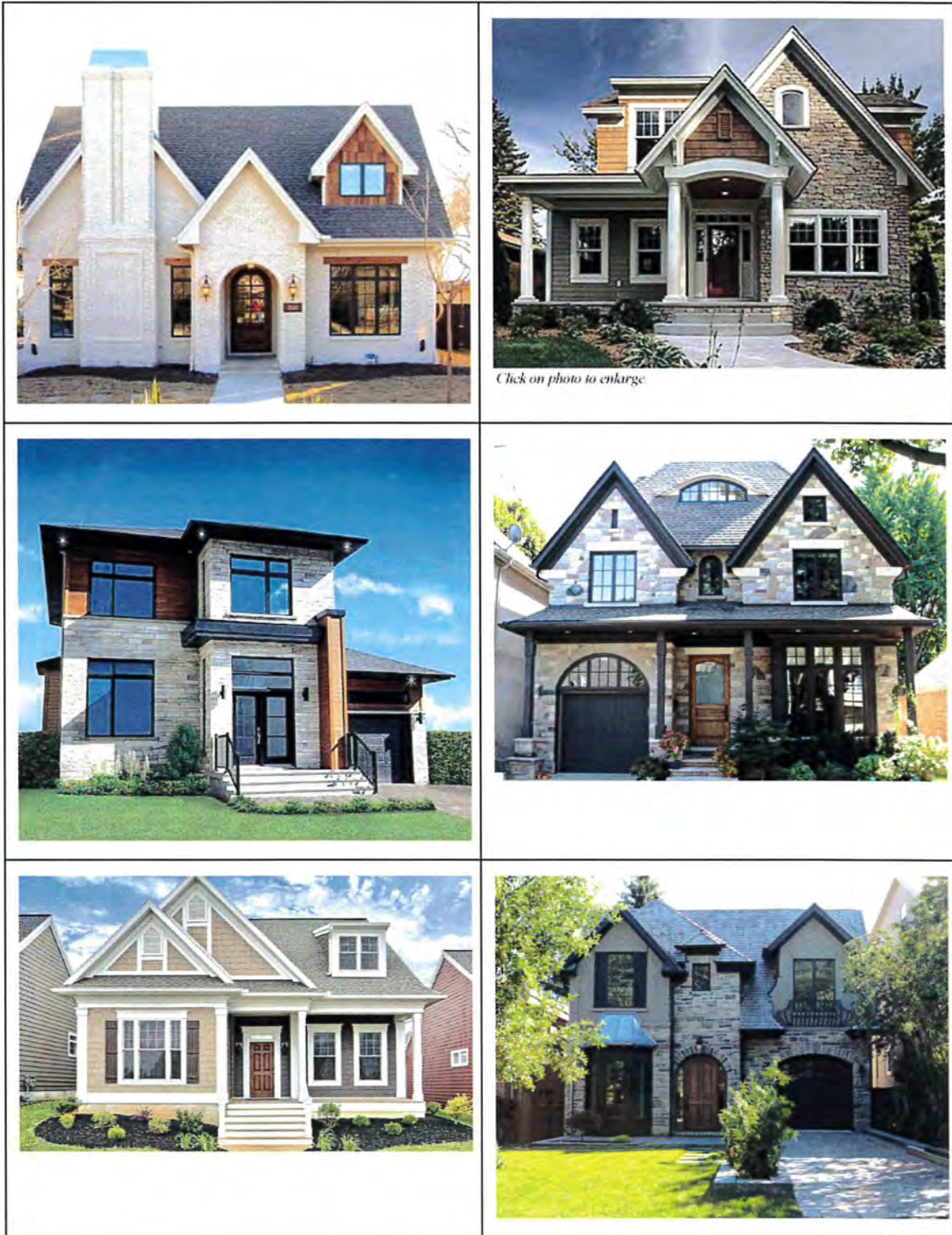


Permitted Zones

DI	CCC	CCA	MR8	MR7
no	no	no	yes	yes

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Examples: MFS



- C.
- D. Lot & Building Requirements - Medium Single Family (MSF).
- E. The following building and lot standards shall be utilized for medium-single family homes:

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F.

	DT Not Permitted	CCC Not Permitted	CCA Not Permitted	MR8 Min.	MR7 Max.	SF6
Lot Area:				4,500 sf	6,500 sf	
Lot Coverage:				no min.	60%	no min.
Lot Width (A):				45'	65'	
Front Setback (B):				7'	20'	20'
Side Setback (C):				7'	no max.	5'
Rear Setback - Home (D):				15'	no max.	20'
Building Width:				60%	90%	
Stories:				1	2	2
Height (E):				11'	25'	11'

1. Lot Access Standards:
 - a. Where an alley is present, Parking and services shall be accessed through the alley.
 - b. Where an alley is not present, Parking and services shall be accessed by a driveway of 8' to 18' wide (at start of driveway/ROW) and at least a 3' landscaped side yard setback.
 - c. On a corner lot without access to an alley, Parking and services shall be accessed by a driveway off of the street not faced by the primary entrance to the building. These driveways may be up to 18' wide (at start of driveway/ROW) with a 3' landscaped side or rear yard setback.
2. Parking Standards:
 - a. Minimum two (2) off-street space per MSF.
 - b. An alley accessed lot must provide Parking in the rear yard. A non-alley accessed lot shall provide spaces in either the side yard behind the building frontage or in the rear yard.
 - c. Parking in the park strip is prohibited.
 - d. Garages (covered Parking) shall be in accordance to Section 12C-1066.10.
3. Service & Utility Standards: Above ground mechanical devices shall be located in the side or rear yard, behind all portions of the principal facade, and shall be fully screened from the street and neighboring properties.
4. Building Frontage Standards:
 - a. At least one (1) functioning entrance to the MSF shall be provided from the street or courtyard.
 - b. MSF on corner lots shall be designed to include windows and at least one (1) vertical plane break in elevation on the second side facing the street.

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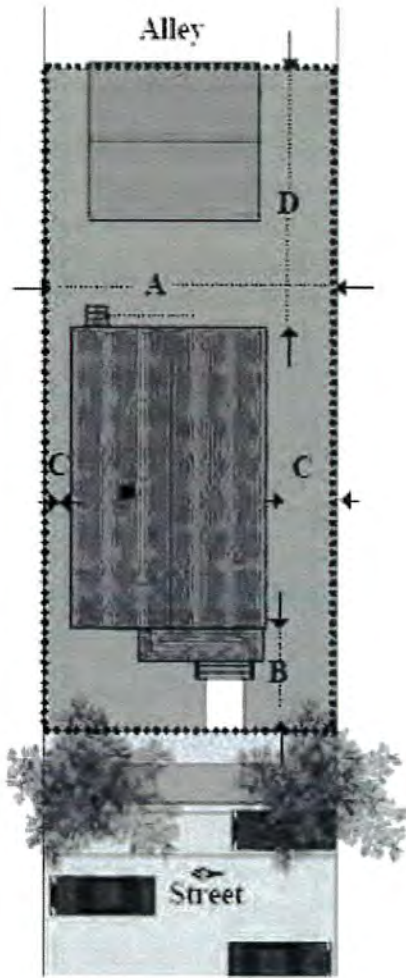
- c. MSF homes should have a porch on the street front side of the building.
- 5. Landscaping shall be required per Section 12C-1067.

Adopted by Ord. 12-09 on 12/19/2012

12C-1066.04 Two - Family Home (TFH) Remove as a type unless a timeless example can be provided, that won't just be a rental unit (ie a senior housing unit)

- A. A detached structure with two units that is massed as a single structure. Yards can range from small to large and units can be side-by-side or stacked. This type of building is also called a Twin-Home.
- B. Typical lot configuration and benchmark examples:

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Permitted Zones

DT	CCC	CCA	MR8	MR7
no	no	yes	yes	yes

- C.
- D. Lot & Building Requirements - Two-Family Home (TFH)
- E. The following building and lot standards shall be utilized for Two-Family Home buildings. The dimensions below represent the whole building (both sides) and whole lot, which can be subdivided. If the Two Family Home lot is subdivided then the lot coverage and lot area is divided by 2.

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F.

	DT	CCC	CCA	Max.	MR8	MR7
	Not Permitted	Not Permitted	Min.		Min.	Max.
Lot Area:			7,000 sf	10,000 sf	8,000 sf	11,000 sf
Lot Coverage:			no min.	60%	no min.	60%
Lot Width (A):			50'	100'	50'	100'
Front Setback (B):			10'	25'	10'	25'
Side Setback (C):			5'	no max.	5'	no max.
Rear Setback - Home (D):			12'	no max.	15'	no max.
Building Width:			60%	90%	60%	90%
Stories:			1	2	1	2
Height (E):			11'	35'	11'	35'

1. Lot Access Standards

- a. Where an alley is present, Parking and services shall be accessed through the alley.
- b. Where an alley is not present, Parking and services shall be accessed by a driveway of 8' to 18' wide at the sidewalk (start of apron), with at least a 3' landscaped side yard setback.
- c. On a corner lot without access to an alley, Parking and services shall be accessed by a driveway off of the street not faced by the primary entrance to the building. These driveways may be up to 18' wide at the top of the apron with a 3' landscaped side or rear yard setback.

2. Parking Standards:

- a. Minimum two (2) off-street space per TFH (per side).

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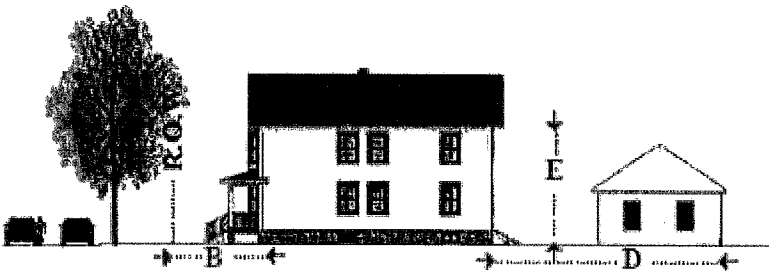
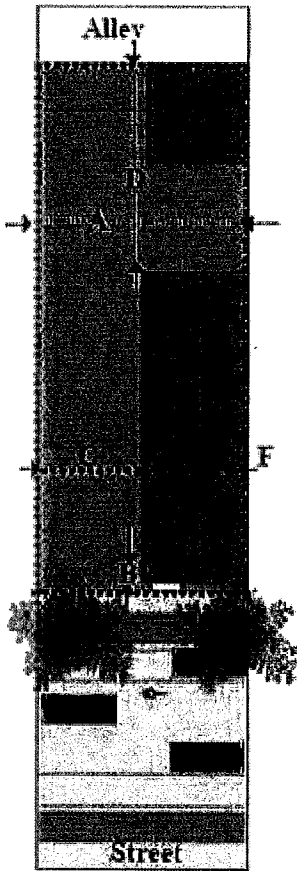
- b. An alley accessed lot must provide Parking in the rear yard. A non-alley accessed lot shall provide spaces in either the side yard behind the building frontage or in the rear yard. Maximum Parking is one (1) space per 400 square feet in the building.
 - c. Parking in the Park strip is prohibited.
 - d. Garages (covered Parking) shall be in accordance to Section 12C-1066.10.
 - e.
3. Service & Utility Standards: Above ground mechanical devices shall be located in the side or rear yard, behind all portions of the principal facade, and shall be fully screened from the street and neighboring properties.
4. Building Frontage Standards:
- a. At least one (1) functioning entrance to each of the TFH shall be provided from the street or courtyard.
 - b. TFH on corner lots shall be designed to include windows and at least one (1) vertical plane break in elevation on the second side facing the street.
 - c. TFH homes should have a porch on the street front side of the building.
5. Landscaping shall be required per Section 12C-1067.

Adopted by Ord. 12-09 on 12/19/2012

12C-1066.05 Townhome (TH)

- A. An attached home with common walls on one (1) or two (2) sides, with no units above or below.
- B. Typical configuration and ~~examples~~

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Permitted Zones

DT	CCC	CCA	MR8	MR7
no	no	yes	yes	yes

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- C. Lot & Building Requirements - Townhome (TH)
- D.

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	DT	CCC	CCA	Max.	MR8	MR7
	Not Permitted	Not Permitted	Min.		Min.	Max.
Lot Area:			2,000 sf	5,000 sf	2,000 sf	5,000 sf
Lot Coverage:			no min.	80%	no min.	80%
Lot Width (A):			20'	50'	20'	50'
Front Setback (B):			5'	25'	5'	20'
Side Setback (C):			5'	no max.	5'	no max.
Rear Setback - Home (D):			15'	no max.	15'	no max.
Building Width:			90%	100%	80%	100%
Stories:			2	3	1	2
Height (E):			20'	35'	11'	35'
Attached Side (F):			Attache d		Attache d	

E. The following building and lot standards shall be utilized for each Townhome unit.

1. Lot Access Standards

- a. Where an alley is present, Parking and services shall be accessed through the alley.
- b. Where an alley is not present, Parking and services shall be coordinated through a rear private drive serving multiple Townhomes. Driveways off of the street should be minimized in quantity and width. Generally drives should be no more than 18 feet in width.

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- c. On a corner lot without access to an alley, Parking and services shall be accessed by a driveway off of the street not faced by the primary entrance to the building. These driveways may be up to 18' wide at the top of the apron with a 3' landscaped side or rear yard setback.
2. Parking Standards:
 - a. Minimum two (2) off-street space per TH.
 - b. An alley accessed lot must provide Parking in the rear yard. A non-alley accessed lot should provide Parking in the rear yard.
 - c. Parking in the Park strip is prohibited.
 - d. Garages (covered Parking) shall be in accordance to Section 12C-1066.10.
3. Service & Utility Standards: Above ground mechanical devices shall be located in the side or rear yard, behind all portions of the principal facade, and shall be fully screened from the street and neighboring properties.
4. Building Frontage Standards:
 - a. At least one functioning entrance to each of the TH shall be provided from the street.
 - b. TH on corner lots shall be designed to include windows and at least one vertical plane break in elevation on the second side facing the street.
 - c. TH homes should have a porch on the street front side of the building.
5. Landscaping shall be required per Section 12C-1067.
6. Maximum units per building shall be four (4).

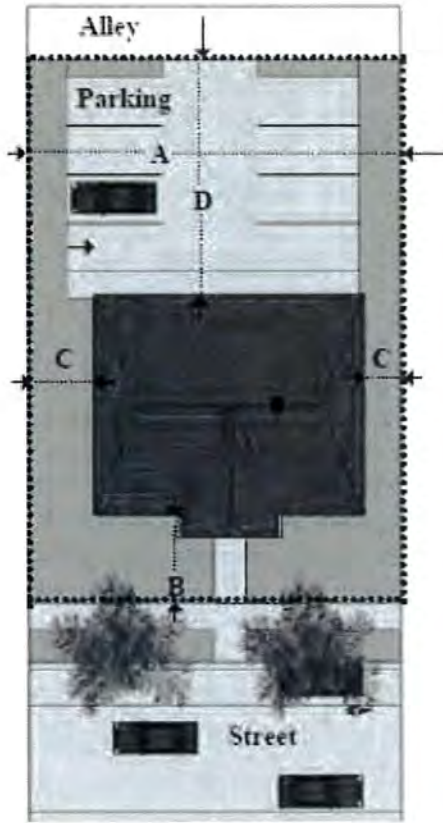
Adopted by Ord. 12-09 on 12/19/2012

12C-1066.06 Multi-Unit House (MUH)

- A. A structure designed to resemble a large house but contains multiple units above and beside each other. This type of structure is commonly called Mansion Apartment. Contains three to four (3-4) units.
- B. Typical configuration and examples:

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Permitted Zones

	DT	CCC	CCA	MR8	MR7
	no	no	yes	yes	no

- E.
- D. Lot & Building Requirements—Multi-Unit House (MUH).
- E. The following building and lot standards shall be utilized for Multi-unit House buildings:
- F.

	DT	CCC	CCA	Max.	MR&	MR7
	Not Permitted	Not Permitted	Min.		Min.	Max.

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Lot Area:			4,000 sf	10,000 sf	5,000 sf	10,000 sf
Lot Coverage:			no min.	90%	no min.	80%
Lot Width (A):			40'	60'	40'	700'
Front Setback (B):			7'	20'	10'	20'
Side Setback (C):			7'	no max.	7'	no max.
Rear Setback—Home (D):			15'	no max.	15'	no max.
Building Width:			70%	90%	60%	80%
Stories:			2	2.5	2	2.5
Height (E):			20'	35'	20'	35'

1. Lot Access Standards:

- a. Where an alley is present, Parking and services shall be accessed through the alley.
- b. Where an alley is not present, Parking and services shall be accessed through a drive 10' to 18' wide, with at least 3' landscaped side yard setback.
- c. On a corner lot without access to an alley, Parking and services shall be accessed by a driveway off of the street not faced by the primary entrance to the building. These driveways may be up to 18' wide at the top of the apron with a 3' landscaped side or rear yard setback.

2. Parking Standards:

- a. Multi-unit house one (1) space per 600 sf of total apartment/condo square footage. Apartment/condo square footage is the total sum of apartments/condo square footage not including club house and etc.
- b. An alley accessed lot must provide Parking in the rear yard. A non-alley accessed lot should provide Parking in the rear yard.
- c. Parking in the Park strip is prohibited.
- d. Garages (covered Parking) shall be in accordance to Section 12C-1066.10.
- e. Surface Parking shall be at minimum 10' from any street right of way (not including alleys) and 5' from any side lot line.

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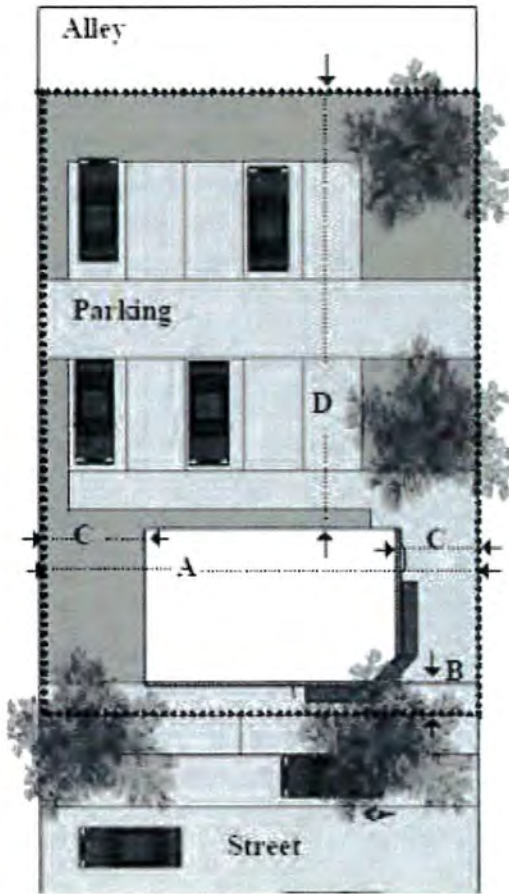
- ~~f. Minimum three (3) bike Parking stalls and one (1) bike rack per building.~~
- ~~3. Service & Utility Standards: Above ground mechanical devices shall be located in the side or rear yard, behind all portions of the principal facade, and shall be fully screened from the street and neighboring properties.~~
- ~~4. Building Frontage Standards:~~
 - ~~a. No more than one (1) entrance to multi-unit houses may be from a street yard.~~
 - ~~b. MH on corner lots shall be designed to include windows and at least one (1) vertical plane break in elevation on the second side facing the street.~~
 - ~~c. Stairways to upper story units must not be on the outside of the building shell and must be internal to the building footprint.~~
 - ~~d. Maximum units per building shall be four (4).~~
- ~~5. Landscape Standards shall be required per Section 12C-1067.~~

Adopted by Ord. 12-09 on 12/19/2012

12C-1066.06 Multi-Unit Building (MUB)

- A. A structure designed to accommodate multiple units above and or beside each other.
- B. Typically contains more than five (5) units.
- C. Typical configuration and examples:

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Permitted Zones

DT	CCC	CCA	MR8	MR7
yes	yes	yes	no	no

- D.
- E. Lot & Building Requirements - Multi-unit Building (MUB)
- F. The following building and lot standards shall be utilized for Multi-unit House Building.
- G.

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	DT	CCC	CCA	Max.	MR8	MR7
	Not Permitted	Not Permitted	Min.		Not Permitted	Not Permitted
Lot Area:			4,000 sf	12,000 sf		
Lot Coverage:			no min.	80%		
Lot Width (A):			90'	200'		
Front Setback (B):*			20'	80'		
Side Setback (C):			15'	no max.		
Rear Setback - Home (D):			15'	no max.		
Building Width:			70%	100%		
Stories:			2	3		
Height (E):			no min.	45'		

H. * 20' max building setback for public street and 80' max building setback for private drive.

1. Lot Access Standards:

- a. Where an alley is present, Parking and services shall be accessed through the alley.
- b. Where an alley is not present, Parking and services shall be accessed through a drive no more than 20' wide.
- c. On a corner lot without access to an alley, Parking and services shall be accessed by a driveway off of the street not faced by the primary entrance to the building. These driveways should be minimized in quantity and width.

2. Parking Standards:

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- a. Apartment Building - 1 (one) space per 600 sf of total apartment square footage. Apartment/condo square footage is the total sum of apartments/condo square footage not including club house and etc.
- b. Minimum required bike Parking is one (1) secured bike stall for every eight (8) off-street Parking stalls.
- c. Surface Parking shall be at minimum 10' from any street right-of-way (not including alleys) and 5' from any side lot line.
3. Service & Utility Standards: Above ground mechanical devices shall be located in the side or rear yard, behind all portions of the principal facade, and shall be fully screened from the street and neighboring properties.
4. Trash containers shall be stored out of public view and be screened from adjacent properties.
5. Building Frontage Standards:
 - a. Entrances to units may be individual entrances, a foyer entrance, or internal corridor entrance from a street yard or courtyard.
 - b. No more than three (3) entrances to multi-unit houses may be from a street yard. MUB on corner lots shall be designed to include windows and at least one (1) vertical plane break in elevation on the second side facing the street.
 - c. Stairways to upper story units must not be on the outside of the building shell and must be internal to the building footprint.
6. Landscaping shall be required per Section 12C-1067.

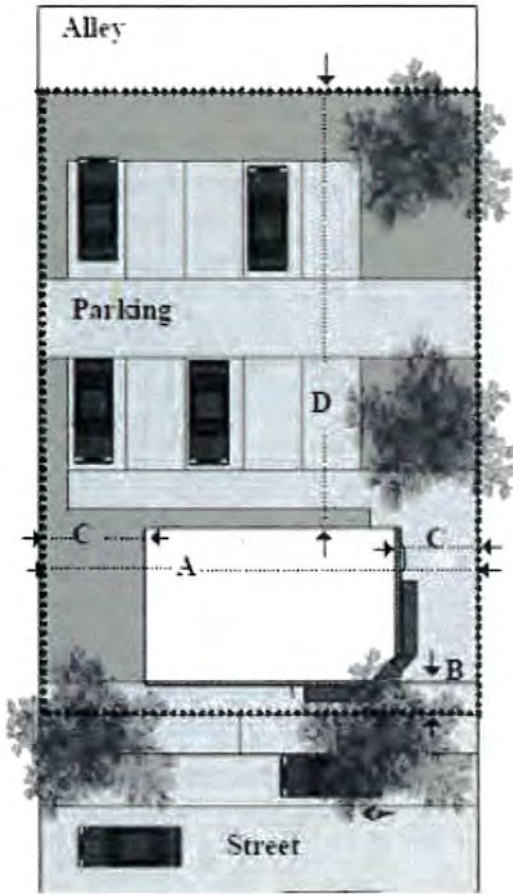
Adopted by Ord. 12-09 on 12/19/2012

Amended by Ord. 16-06 on 9/7/2016

12C-1066.07 Classic Commercial Building (CC)

- A. A structure designed to accommodate one (1) or more businesses/users. Commercial use is required at the first (1) floor. Residential or commercial uses (including office) are encouraged on the second (2) floor and third (3) floor.
- B. Typical configuration and examples:

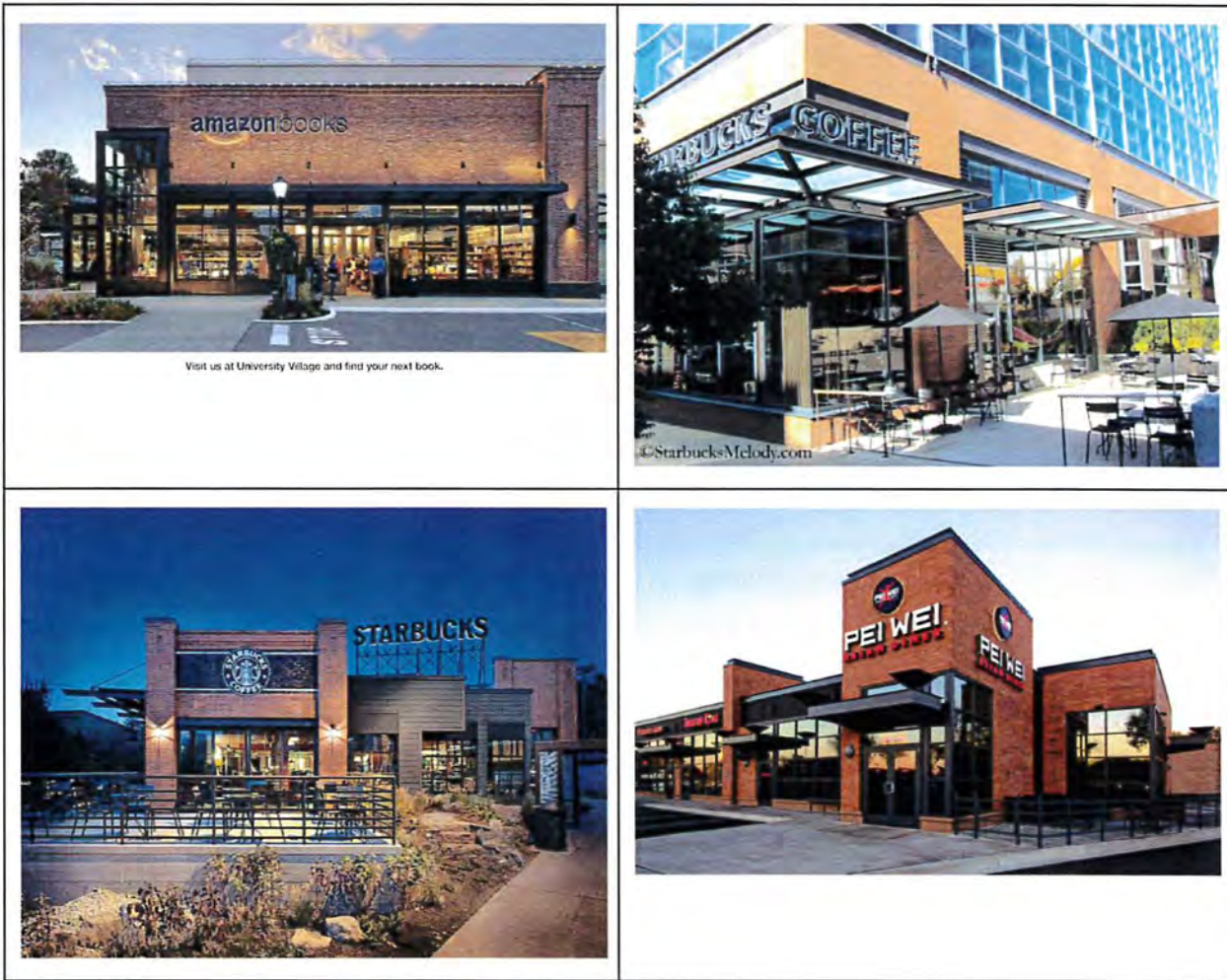
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Permitted Zones

DT	CCC	CCA	MR8	MR7
yes	yes	yes	no	no

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- C. Lot & Building Requirements-Classic Commercial Building (CC).
- D.
- E. The following building and lot standards shall be utilized for the Classic Commercial Building:

	DT	Max	CC	Max	CC	Max	MR8	MR7	SF6
	Min		C		A		Not Permitted	Not Permitted	
			Min.		Min.				
Lot Area:	4,000 sf	no max.	4,000 sf	no max.	4,000 sf	no max.			

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Lot Coverage:	no min.	95%	no min.	85%	no min.	85%			
Lot Width (A):	30'	100'	50'	100'	30'	100'			
Front Setback (B):	3'	15'	3'	15'	3'	15'			
Side Setback (C):	0'	16'	3'	16'	3'	16'			
Rear Setback - Home (D):	10'	no max.	15'	no max.	15'	no max.			
Building Width:	70%	95%	60%	95%	70%	95%			
Stories:	1.5	3	2	3	2	3			
Height (E):	22'	45'	11'	45'	11'	35'			

1. Lot Access Standards:
 - a. Where an alley is present, Parking and services shall be accessed through the alley.
 - b. Where an alley is not present, Parking and services shall be accessed through a drive no more than 20' wide.
 - c. Coordinated driveway access and cross access to multiple sites should be provided where practical.
2. Parking Standards:
 - a. All lots shall provide off-street Parking spaces in the rear yard or side yard. Rear yard is preferred.
 - b. Commercial Parking requirements are per City standards.
 - c. Residential: Minimum one (1) off-street space per unit. Maximum one (1) off-street space per 400 square feet of residential living
 - d. Required bike Parking is one (1) secured bike stall for every ten (10) off-street Parking stalls.
 - e. Surface Parking shall be at minimum 10' from any street right-of-way (not including alleys) and 5' from any side lot line.
3. Service & Utility Standards: Above ground mechanical devices shall be located in the side or rear yard, behind all portions of the principal façade, and shall be fully screened from the street and neighboring properties. Trash containers shall be stored out of public view and be screened from adjacent properties.
4. Building Frontage Standards:

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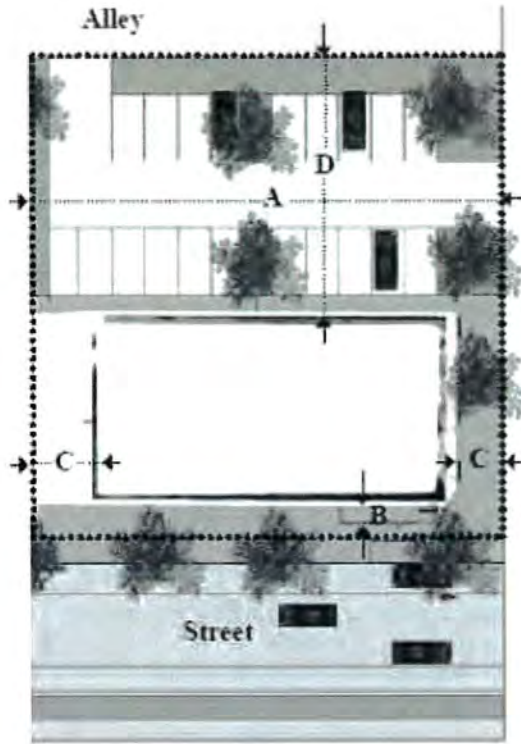
- a. At least one (1) entrance to each ground floor unit must be from a street yard.
 - b. Stairways to upper story units must not be on the outside of the building shell and must be internal to the building footprint.
 - c. Street walks and planter strips shall conform to Complete Streets Section 12C-1064.
5. Landscaping shall be required per Section 12C-1067.

Adopted by Ord. 12-09 on 12/19/2012

12C-1066.08 City Commercial Building (CCB)

- A. A larger scale structure designed to accommodate a mix of uses. Buildings share common walls. Floor to ceiling heights are taller. Commercial use is required on first (1) floor and residential or commercial uses are encouraged on second (2) or third (3) floor.
- B. Typical lot configuration and examples:

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Permitted Zones

DT	CCC	CCA	MR8 /MR7
yes	yes	yes	no

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C. City Center Commercial Building.

D. The following building and lot standards shall be utilized for City Center Commercial Buildings:

E.

	DT	Max.	CCC	Max.	CCA	Max.	MR8	MR7
	Min.		Min.		Min.		Not Permitted	Not Permitted
Lot Area:	2,000 sf	no max.	4,000 sf	no max.	4,000 sf	no max.		

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Lot Coverage:	no min.	100%	no min.	85%	no min.	85%		
Lot Width (A):	30'	100'	50'	250'	30'	100'		
Front Setback (B):	0'	1'	0'	25'	0'	25'		
Side Setback (C):	0'	20'	0'	20'	0'	20'		
Rear Setback - Home (D):	10'	no max.	15'	no max.	15'	no max.		
Building Width:	70%	100%	60%	95%	60%	95%		
Stories:	1.5	3	2	3	2	3		
Height (E):	22'	45'	15'	45'	15'	35'		

1. Lot Access Standards

- a. Where an alley is present, Parking and services shall be accessed through the alley.
- b. Where an alley is not present, Parking and services shall be accessed through a drive no more than 20' wide.
- c. Coordinated driveway access and cross access to multiple sites should be provided where practical.

2. Parking Standards:

- a. All lots shall provide off-street Parking spaces in the rear yard (preferred) or side yard.
- b. Commercial Parking requirements are per City standards.
- c. Residential: Minimum one (1) off-street space per unit. Maximum one (1) off-street Parking per 400 sf.
- d. Surface Parking shall be at minimum 5' from any street right-of-way or side lot line and 10' behind the street yard.
- e. At least half of the required Parking shall be on-site.
- f. Minimum required bike Parking is one (1) secured bike stall for every ten (10) off-street Parking stalls. This applies for the first 160 auto Parking stalls.
- g. Surface Parking shall be at minimum 10' from any street right-of-way (not including alleys) and 5' from any side lot line.

3. Service & Utility Standards: Above ground mechanical devices shall be located in the side or rear yard, behind all portions of the principal facade, and shall be fully screened

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from the street and neighboring properties. Trash containers shall be stored out of public view and be screened from adjacent properties.

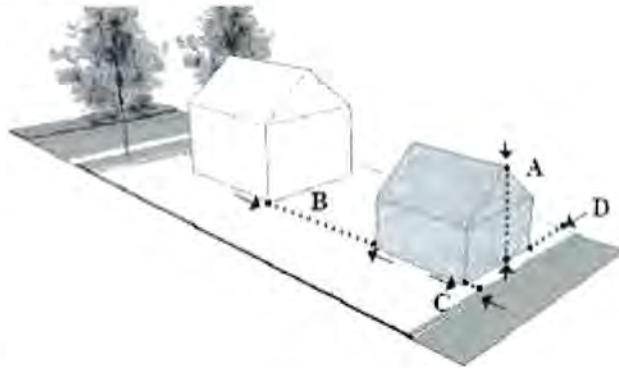
4. Building Frontage Standards:
 - a. At least one (1) entrance to each ground floor unit must be from a street or side yard.
 - b. Stairways to upper story units must not be on the outside of the building shell and must be internal to the building footprint.
5. Landscaping shall be required per section 12C-1067.

Adopted by Ord. 12-09 on 12/19/2012

12C-1066.9 Garages

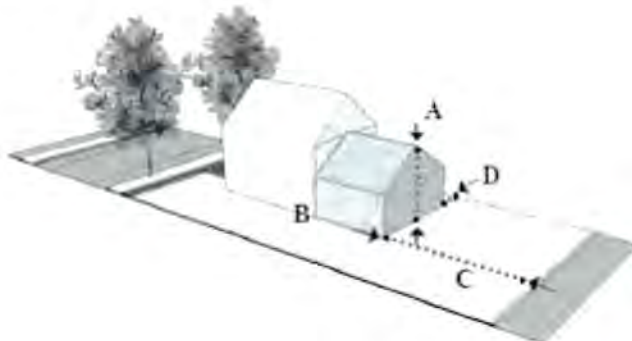
Garage Parking Configurations shall have the Parking structure located only in the rear of the lot. The various standard garage configurations are as follows. If an alley is present, access to the covered Parking / Garage structure must be from the alley.

- A. Detached Rear Garages. The standards for covered detached rear parking structures shall be as follows:



- B.
 1. MI Maximum height (A): Same or less than primary structure
 2. Minimum clear between garage and building (B): 15% of rear yard
 3. Minimum rear (C): 4' or up to 0' if 16' wide alley is present
 4. Minimum side (D): Same as building type
 5. Maximum area: 800 sf footprint

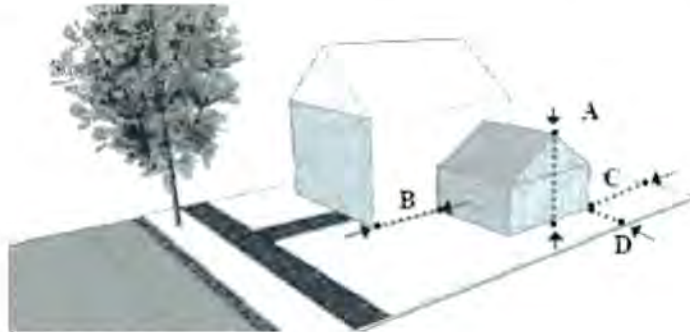
- C. Attached Rear Garages. The standards for covered, attached rear, parking structures shall be as follows:



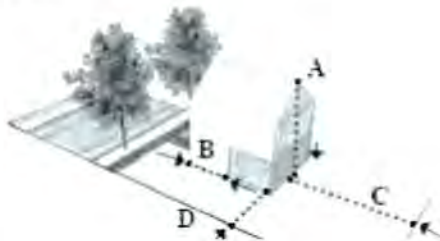
- D.

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1. Maximum height (A): Attached garage may be no taller than the building to which it is attached.
 2. (B) building is attached to the garage
 3. Minimum rear (C): Same as building type
 4. Minimum side (E): Same as building type
 5. Maximum area: 800 sf footprint
 6. Attached garage may be no wider than the building to which it is attached.
- E. Attached Side Garages. The standards for covered, attached, side parking structures shall be as follows:



- F.
1. Maximum height (A): Attached garage may be no taller than the building to which it is attached.
 2. Minimum setback from front facade (B): 4'
 3. If the garage opening is on the side (side loaded) the setback may be equal to the front facade, the garage facade must have matching architecture as the building.
 4. Minimum rear (C): Same as building type
 5. Minimum side (D): Same as building type
 6. Maximum area: 800 sf footprint
- G. Tucked Under Attached Garages. The standards for covered Parking structures shall be as follows:



- H.
1. IM Maximum height (A): Attached garage may be no taller than the building to which it is attached.
 2. Minimum setback from front facade (B)
 3. If the garage opening is on the side (side loaded) the setback may be equal to the front facade, with matching architecture as the building.
 4. Minimum rear (C): Same as building type
 5. Minimum side (E): Same as building type
 6. Maximum area: 800 sf footprint

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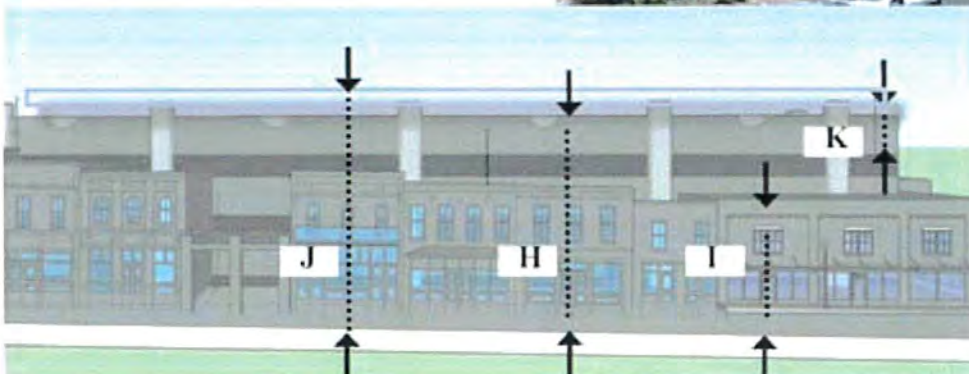
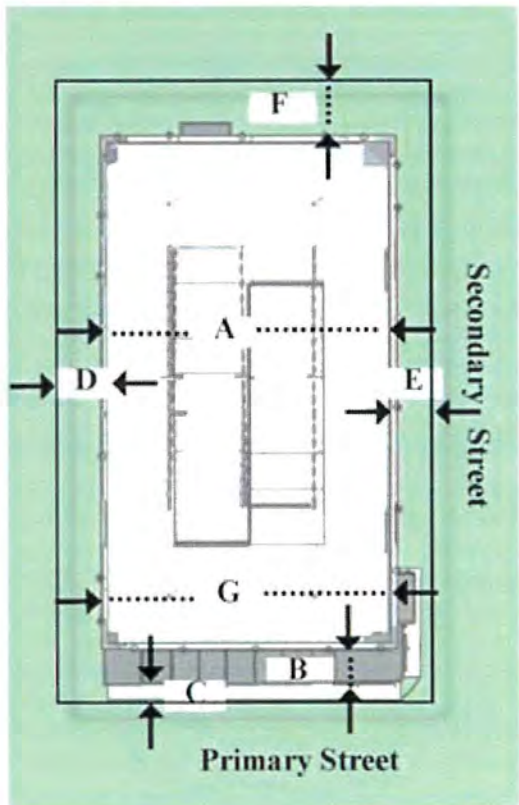
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7. Tuck-under Parking shall be contained within the footprint of the building type.

Adopted by Ord. 12-09 on 12/19/2012

12C-1066.10 Parking Structures (PS)

- A. A larger scale structure designed to accommodate parking. Parking structures are an important part of urban development.
- B. Parking Structure Building



C.
D.

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	DT Min	Max	CCC Min.	Max	CCA Min.	Max	MR Not Permitted
Lot Area:	No Min	No Max	No Min.	No Max	No Min.	No Max	
Lot Coverage:	No Min	90%	No Min.	90%	No Min.	90%	
Building Width (A):	50'	330'	50'	330'	30'	330'	
PS Front Setback (B):	20'	No Max	20'	No Max	20'	No Max	
Frontage Building Setback (C):	0'	5'	5'	20'	5'	50'	
Non Street Side Setback (D):	15'	No Max	15'	No Max	15'	No Max	
2200 N Setback (for PS) (E):	20'	No Max	20'	No Max	20'	No Max	
2200 N Setback (for frontage building) (E):	0'	10'	0'	10'	0'	10'	
Rear Setback - (F):	15'	No Max	15'	No Max	15'	No Max	
Building Frontage (G):	70%	100 %	60%	100 %	60%	100 %	

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PS Height to top of parapet wall (H):	22'	32'	15'	32'	15'	32'	
Frontage Building Height (I)	22'	45'	15'	45'	22'	35'	
PS Height (for solar and weather protection structure) (J)	22'	45'	15'	45'	22'	40'	
Parapet Wall Height (K)	5'	10'	5'	10'	5'	10'	

- A. The following building and lot standards shall be utilized for Parking Structure:
1. Parking Structures facing the primary streets of 300 East or 200 East shall have Frontage Buildings on the street side of the building.
 2. The Parking Structure architecture facing public streets shall be designed to match the architecture of the Frontage Buildings. The design of the Parking Structure should look like an extension of the Frontage Buildings and designed as a single project. The design of the Parking Structure shall be integrated with the Frontage Buildings through building materials, colors, architecture style, design and massing.
 3. Frontage Buildings shall be either Classic Commercial or City Commercial Buildings.
 4. Parapet walls of Parking Structure shall have variety in: colors, building materials, and architectural detailing. The top of the Parapet wall shall also have a minimum 1 foot vertical elevation change and a minimum horizontal change every 20 feet.
 5. Bike Parking:
 - a. Minimum required bike Parking is one (1) secured bike stall for every ten (10) off-street Parking stalls. This applies for the first 160 auto Parking stalls.
 6. Service & Utility Standards: Above ground mechanical devices shall be located in the side or rear yard, behind all portions of the principal facade, and shall be fully screened from the street and neighboring properties. Trash containers shall be stored out of public view and be screened from adjacent properties.
 7. Building Frontage Standards:
 - a. At least one (1) entrance to each ground floor unit must be from a street or side yard per building frontage.
 - b. Stairways to upper story units may be on the outside of the building shell. **Stair towers shall be properly designed so they can be used effectively to break up a long elevation and also could be made into a tower feature at the corners.**
 8. Frontage Buildings shall be required to extend or wrap corners of the Parking Structure on public streets with a minimum distance of 25 feet.
 9. Where a Parking Structure is on a corner location of a primary street (300 E or 200 E) the Parking Structure shall have a corner tower or architecture gateway feature on the building.
 10. Where a Parking Structure faces secondary streets (2200 North and other non primary streets) a minimum of 75% vegetation coverage screening is required. The 75% screening or vegetation plantings shall be combination of trees and shrubs and shall be

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designed to visually screen 75% of the building after 10 years of growth. Developer to provide drawings (plan view and elevations) that show plant coverage at 10 years.

11. Landscaping shall be required per section 12C-1067.
12. High-quality exterior finishes must be used on all sides of a Parking Structure. High quality exterior finish materials are defined as brick, fiber-cement or similar lap siding, glass, unpainted sandblasted concrete, steel, or real stone. Brick is to be used on 75% (minimum) of all non-glass areas of each facade. All awnings are to be metal or glass (no fabric).

Adopted by Ord. 16-06 on 9/7/2016

12C-1067 Landscaping

12C-1067.01 Landscape Design Standards And Guidelines

The landscaping requirements specified are intended to foster aesthetically pleasing development which will protect and preserve the appearance, character, health, safety and welfare of the community. These regulations are also intended to preserve, enhance and expand the urban forest and promote the prudent use of water and energy resources. This section 12C-1067 applies to the following zones: CCC, DT, CCA, SF6, MR8 and MR7.

A. Examples



B.

- C. Landscape Plan Required: A landscape plan shall be required whenever landscaping or alteration of landscaping is required by this title. Such landscape plan shall be drawn in conformance with the requirements specified in this section. Landscape plans must be approved by the City prior to the issuance of a building permit. Landscape plans for planned developments or conditional uses, or other uses requiring site plan review approval shall be reviewed and approved by the City. All commercial and residential projects are required to submit landscape plans. For all individual lots with Small Single Family, Medium Single Family and Two Family Home, the owner or builder is required to submit landscape plans to the City meeting the requirements from 12C-1067 for only the front yard. Landscape plans for Small Single Family, Medium Single Family and Two Family Homes are the only building lots that do not require a stamp by a Utah Landscape Architect, the remaining building types (TH, MUH, MUB, CC, and CCB) require a Utah Landscape Architect stamp on the landscape plans.

D. Landscape Concept Plan.

1. Landscape Concept Plan(s) are required to be submitted with preliminary plats or preliminary architectural drawings (whichever comes first).
2. Landscape Concept Plan(s) must show location of buildings, drives, streets, walks, Parking areas, concept plants and general nature of the landscape.

E. Final Landscape Plan(s).

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1. All landscape plans submitted for approval shall contain the following information, unless specifically waived by the City:
2. The location and dimensions of all existing and proposed structures, property lines, easements, Parking lots and drives, roadways and rights of way, sidewalks, bicycle paths, ground signs, bicycle Parking areas, fences, freestanding electrical equipment, tot lots, and other recreational facilities.
3. The location, quantity, size and name, both botanical and common names, of all proposed plants;
4. The location, size and common names, of all existing plants including trees and other plants in the Parkway, and indicating plants to be retained and removed.
5. The location of existing buildings, structures and plants on adjacent property within twenty feet (20') of the site.
6. Location and heights of fences and retaining walls proposed on the site.
7. Irrigation plan(s) must also be submitted with Final Landscape Plans. Irrigation plan must be stamped by a Certified Irrigation Designer (CID), Professional Engineer (PE), or Licensed Landscape Architect (SSF, MSF and TFH excluded). Irrigation plans must demonstrate water conservation.
8. Final Landscape Plan(s) for all commercial, ~~residential development~~, TH, MUH, MUB, CC, and CCB buildings shall be stamped by a Utah Licensed Landscape Architect and comply with all the requirements of this section and state codes.
9. Summary data indicating the area of the site in the following classifications: Total area and percentage of the site in landscape area, Total area and percentage of the site in turf grass area, and Total area and percentage of the site designed as water-wise plantings.
10. Final Landscape Plan(s) must also include a statement demonstrating how the design and installation will conserve water. All exterior landscaping shall be required to utilize secondary water as the source for irrigation water, and not culinary, unless secondary water is unavailable.

Adopted by Ord. 12-09 on 12/19/2012

12C-1067.01 Landscape Design Standards And Guidelines

- A. Landscape plans shall be prepared based on the following design standards and guidelines. Design standards are numerically measurable design requirements that can be definitively evaluated for compliance. Design guidelines are not precisely measurable, but compliance can be determined through the evaluation process of landscape plan review. The evaluation and approval of landscape plans shall be based on compliance with both the design standards and guidelines.
- B. Design Standards at Time of Planting.
 1. Deciduous Trees: All deciduous shade trees shall have a minimum trunk size of two inches (2") in caliper, unless otherwise specified by the City. For residential building lots with SSF, MSF and TFH homes twenty gallon (20) container trees are approved.
 2. Evergreen Trees: All evergreen trees shall have a minimum size of five feet (5') in height, unless otherwise specified City.
 3. Ornamental Trees: All ornamental trees shall have a minimum trunk size of one and one-half inches (1-1/2") in caliper, unless otherwise specified.

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4. Shrubs: All shrubs shall have a minimum height or spread of eighteen inches (18") depending on the plant's natural growth habit, unless otherwise specified. Plants in five (5) gallon containers will generally comply with this standard.
5. Perennials and Grasses: Minimum perennial and grass size generally should be one (1) gallon.
6. Deciduous street trees shall be planted a minimum of every 25' to 30' in the park strip or in a tree grate or landscaping area adjacent to the street. The minimum street tree size is a two (2") caliper trunk and the street tree selection shall be according to the City approved street tree list.
7. For projects in the City Center the following landscape requirements apply. For SSF, MSF and TFH building homes/lots these requirements apply to the front yards:
 - a. Minimum of fifteen (15) trees shall be required per one (1) acre of gross land (excluding street trees).
 - b. Minimum fifty (50) shrubs shall be required per one (1) acre of gross land. Two (2) perennials equal one (1) shrub for planting requirements.
 - c. For Residential projects (twin homes up to apartment buildings) the maximum percentage of landscape area that may be turf grass is seventy percent (70%) and the remaining landscape area should be planter bed areas. For Commercial and Mixed-Use projects the maximum percentage of landscape area that may be turf grass is sixty percent (60%) and the remaining landscape area should be planter bed areas. Common Greens, Town Squares and sports fields do not have a limited turf grass requirement. The remaining landscape percentage should be designed/installed as a waterwise landscape.

C. Design Guidelines.

1. Scale and nature of landscaping material: The scale and nature of landscaping materials shall be appropriate to the size of the structures. Large scale buildings, for example, should generally be complemented by larger scale plants.
2. Plants shall be selected for form, texture, color, pattern of growth and adaptability to local conditions.
3. Evergreens should be incorporated into the landscape treatment of a site, particularly in those areas where screening and buffer is required.
4. Softening of Walls and Fences: Plants shall be placed intermittently against long expanses of building walls, fences, and other barriers to create a softening effect.
5. Planting beds may be mulched with bark chips, decorative stone, or similar materials. Mulch shall not be used as a substitute for plants.
6. Turf Grasses: Turf grasses should be used in areas with less than a thirty percent (30%) slope to prevent the runoff of irrigation water.
7. Water Conservation: Landscape design pursuant to the requirements of this chapter must recognize the climatic limitations of the Cache Valley area and the need for water conservation. While irrigation systems are required for certain landscape areas, and may be desirable for other applications, all irrigation systems shall be designed for efficient use of water.
8. Detention/Retention Basins and Ponds: Detention/retention basins and ponds shall be landscaped. Such landscaping may include shade and ornamental trees, evergreens, shrubbery, hedges, turf, ground cover and/or other plant materials.

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9. Preservation of Existing Plants: Existing plants should be incorporated into the site and landscape design, except when disease, age, prior damage, new building footprint and other circumstances require removal or when removal is approved by the City.

Adopted by Ord. 12-09 on 12/19/2012

12C-1068 Lighting

The intent of lighting standards is to provide the necessary lighting to ensure pedestrian safety, night vision for pedestrians and automobiles, add aesthetic value to the city appearance, and not create or cause excessive glare onto adjacent properties. The light fixtures and placement will add to the character of the community and neighborhood.

Examples:



- A. Site Lighting Plan: A lighting plan shall be required as part of the Development Plan and Agreement for all projects in the City Center.
- B.
- C. Lighting Standards:
 1. Lighting height and location shall be designed to illuminate the site only. Light cutoffs are required to prevent spillover of direct light.
 2. Lighting shall be provided at consistent levels, with gradual transitions between maximum and minimum levels of lighting and between lit areas and unlit areas. Highly contrasting pools of light and dark areas shall be avoided.
 3. Up-lighting is not permitted, except as approved through design review for highlighting signature landscape features or building elements.
 4. Pedestrian street lights or lampposts are required within the following Zones: Downtown (DT), City Center Adjacent (CCA), City Center Commercial (CCC) and Mixed Residential (MR8 & MR7) and **(SF6)**.
 5. Pedestrian street lights or lampposts shall be mounted between 8' to 16' above grade to provide continuous illumination of all street sidewalks according to the Development Plan and Agreement.
 6. Pedestrian street lights or lampposts shall reflect the architectural design characteristics of the surrounding area.
 7. Exterior building lights affixed to buildings on street-front elevations shall be mounted between 6' and 14' above adjacent grade.
 8. HID or fluorescent tube lights (except compact fluorescent bulbs) are not permitted as exterior building lights, **only LED lights, or lights of similar efficacy shall be allowed.**

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9. Floodlights or directional lights permitted for the lighting of alleys, Parking garages and outdoor working areas must be shielded or directed to prevent the source of light (bulb or lamp) from being seen from adjacent properties or public rights-of-way.
10. All City Center lighting shall follow the lighting requirements set forth in the Design Standards Technical Manual.

Adopted by Ord. 12-09 on 12/19/2012

12C-1069 Retention And Maintenance Guarantees And Covenants

Adequate guarantees shall be provided for the permanent retention and maintenance of all privately owned Open Space Areas, which shall include but not be limited to landscaping, lighting, and other improvements to or the existing status of such Open Space Areas, created and/or maintained pursuant to development plans approved pursuant to this City Center Code. The term "Open Space Areas" shall mean and refer to all privately owned property within an approved development, which requires new improvements or the maintenance of existing improvements, whether referred to as open space, common areas and/or limited common areas, retention/detention basins or ponds, or otherwise. Final plan approval will not be granted until all required guarantees have been submitted to and approved by the Planning Commission. Said open space guarantees may include the following:

- A. The City may require the developer to include with recorded Covenants, Conditions, and Restrictions (CC&Rs), provisions which will guarantee the retention and maintenance of the Open Space Areas and which grant beneficial use rights in the Open Space Areas to all owners or occupants of the land within the development or the City may require the creation of a corporation or other entity, to own title to the Open Space Areas and to grant beneficial use rights in the Open Space Areas to all owners or occupants of land within the development.
- B. The developer shall be required to develop and provide for the maintenance of all Open Space Areas, unless part of or all of the Open Space Areas are contiguous to and are made a part of an existing public park, and the City accepts dedication and approves the annexation of the Open Space Areas to said public park.
- C. In the case of private ownership of Open Space Areas, the Open Space Areas to be reserved shall be protected against building development by conveying to the City as part of the condition of project approval, an open space easement over such Open Space Areas, restricting the area(s) against any future building or use, except as approved on the project plan or by the City, upon application and approval pursuant to City Code.
- D. The care and maintenance of the Open Space Areas may be ensured by the developer's establishment of a private association, corporation, or other entity responsible for such maintenance which shall levy the cost thereof as an assessment on the property owners within the development. Ownership and tax liability of private Open Space Areas shall be established in a manner acceptable to the City and made a part of the conditions of the final plan approval.
- E. Maintenance of Open Space Areas shall be managed by a person or persons, a partnership, a corporation or other entity in which there is adequate expertise and experience in property management to assure the maintenance is accomplished efficiently and at a high standard of quality.
- F. The following provisions shall apply to each development for which the developer establishes a private association, corporation, or other entity (hereafter "homeowners' association" or "HOA") which holds the title to and is responsible for Open Space Areas and the associated maintenance

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and costs. The HOA shall be formed and operated under the following or reasonably similar provisions:

1. The developer shall provide Covenants, Conditions and Restrictions (CC&Rs) of the HOA, including its bylaws, articles of incorporation and methods for maintaining the Open Space Areas. The CC&Rs shall be reviewed and approved in content and form by the City. Acceptance of the CC&Rs by the City will be contingent upon meeting the intent and conditions required by this code. The CC&Rs shall be approved by the City prior to filing the final plat. The CC&Rs shall be recorded at the county recorder's office concurrently with the filing of the final plat.
2. The HOA shall be organized by the developer and be operated with financial subsidization by the developer, before the sale of any lots, or transfer of any other approved property divisions, within the development.
3. Membership in the HOA shall be automatic (mandatory) for all purchasers of homes, lots or other property interests in the development and their successors. The conditions and timing of transferring control of the HOA from developer to homeowners or other property owners shall be identified in the CC&Rs.
4. The HOA shall be responsible for maintenance of insurance and taxes on Open Space Area(s).
5. The members of the HOA shall share equitably the costs of maintaining and developing all Open Space Areas. Fees shall be determined by the HOA and assessed and deposited in an appropriate HOA account.
6. The developer shall endow the HOA with funds equivalent to ten percent (10%) of the development cost for all Open Space Areas, which shall be used by the HOA to operate, maintain and insure the HOA meets its obligations for the first year that the association begins to operate independently of the developer, unless other appropriate funding is supplied by the developer and approved by the City. Funds shall be deposited in the HOA account in the name of the HOA within ten (10) days after the day on which the HOA begins to operate independently of the developer.
7. All improvements to the Open Space Areas, held in common or intended to be held in common by the HOA, shall be installed, completed and accepted prior to the beginning of the second phase of construction or eighteen (18) months after completion of the public improvements in the first phase of the development, whichever date occurs first; or if the project is not phased, prior to sale of all lots or other approved property divisions or eighteen (18) months after completion of the public improvements in the development, whichever date occurs first. If phasing of the improvements to the Open Space Areas is approved for the developer, all incomplete improvements for the Open Space Areas shall be secured through a bond or other security posted by the developer, in the same manner as set forth in Chapter 12D-500 of the North Logan City Code.
8. The HOA shall have a qualified person or persons or shall hire a qualified property manager or company to administer and maintain all Open Space Areas.
9. Upon application by the developer, with the recommendation of the Planning Commission and the permission of the City Council, an HOA may transfer title to the Open Space Areas, and/or its maintenance responsibilities, to a private nonprofit organization, among whose purposes it is to conserve undivided land, such as the Open Space Areas; provided that:

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- a. The organization is acceptable to the Planning Commission and the City Council, and is a bona fide conservation organization with perpetual existence;
 - b. The conveyance contains appropriate provision for proper reverter or retransfer in event that the organization becomes unwilling or unable to continue carrying out its functions; and
 - c. A maintenance agreement acceptable to the Planning Commission and the City Council is entered into by the developer and the organization.
10. In the event Open Space Areas are included in approved development plans, the following notation shall be recorded on the face of any final plat or shall be included in the Development Agreement, which shall be recorded, or the notation shall otherwise be included as part of the written guarantees and covenants required by this City Center Code for development approval:
11. North Logan City shall have the right, but not the duty, to require, ~~and if necessary, perform~~ or cause to be performed, at the expense of the owner of the Open Space Areas and other private areas and facilities, including clubhouse, pool and other recreation facilities, (HOA, hereafter), all landscaping, snow removal, and other upkeep and maintenance services, as applicable, within the Open Space Area(s), if the HOA fails to adequately perform such tasks. ~~The City may take these actions when asked to assume responsibility for such upkeep and maintenance tasks by the HOA, with City Council approval, and the City Council may also take such actions when it determines the need based on a pattern of neglect and lack of maintenance and after meeting the procedures outlined in the Covenants, Conditions and Restrictions (CC&Rs), Development Agreement or other applicable document, recorded concurrently with this final plat, Development Agreement or other document. In the event North Logan City exercises this right, the City shall be entitled to assess and collect the necessary HOA fees and recover any associated costs and attorney fees. This notation shall not be amended or deleted without the approval of North Logan City.~~
12. The wording of the stated notation may be modified by the Planning Commission and/or City Council, as necessary to make it specific to the project under consideration for approval.
- 13.
- 14.
15. The HOA or other owner of the Open Space Areas shall be responsible for the assessment and collection of all funds required for the operation and maintenance of the Open Space Areas and the physical improvements to the Open Space Areas.
16. In the event that the HOA, or any successor entity, at any time after establishment of the development, shall fail to maintain the Open Space Areas in reasonable order and condition in accordance with the development plan and customary practices for the type of real property and improvements involved, or if the City shall receive notice of three (3) reasonable complaints concerning upkeep of the Open Space Areas or structures from members of the HOA or appropriate City officials or employees, or notice of insurance default on any portion of the property in which the HOA has secured, or is required to secure, an insurance policy, the HOA shall be deemed in default of the

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CCRs, the Development Agreement or other binding document and the CCRs, Development Agreement or other binding document shall provide for the City to serve written notice upon the agent or officer of the HOA, upon the members of the HOA, or other designated agent of the development, setting forth the manner in which the HOA or other entity has failed to maintain the Open Space Areas and directing the HOA or other entity to cure the failure. In the event the HOA or other entity fails to remedy the failure to maintain the Open Space Areas and improvements, within thirty (30) days after the date of the notice, the City shall ~~have the authority to assume the position of the HOA and its Board of Trustees or other entity for the purposes of assessing and collecting the funds necessary from the members of the HOA or other entity to either perform the necessary upkeep and maintenance services and be reimbursed for all expenses incurred, including reasonable attorney's fees and costs, or to hire a designate a private property management company to do so, and to take all other actions necessary and required to maintain said Open Space Areas, at the expense of the HOA and its members or other entity.~~

Adopted by Ord. 12-09 on 12/19/2012

FURTHERMORE BE IT ORDAINED by the City Council of the City of North Logan, Utah as follows:

SECTION II: PARCEL REZONE AND CHANGE TO THE ZONING MAP

1. The real properties with the following tax identification numbers located in North Logan, Utah: 04-084-0033 04-084-0021 04-084-0046 04-084-0023 which are currently zoned Mixed Residential 7 (MR7) zones are hereby rezoned as Single Family (SF6) Zone.
2. The real property with the following tax identification number located in North Logan, Utah: 04-060-0027 which is currently zoned Mixed Residential 8 (MR8), are hereby rezoned as Single Family (SF6) Zone.
3. A map reflecting the areas rezoned will be changed in the existing Ordinance and incorporated therein, The zoning map for the North Logan, Utah Land Use (Zoning) Ordinance, is hereby amended to reflect the change authorized herein.

SECTION III: ADDITION OF SF6 COLUMN TO CHAPTER 12C-1001. Permitted and Conditional Uses in Zones.

A column SF6 is to be added to the Permitted and Conditional Uses in Zones with the same permitted and conditional uses allowed in the MR7 and MR8 columns.

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SECTION III: CONFLICT. To the extent of any conflict between this ordinance and any other North Logan City ordinance(s) or regulation(s), the provisions of this Ordinance shall be controlling.

SECTION IV: SEVERABILITY. The sections, paragraphs, sentences, clauses, and phrases of this Ordinance are severable. If any such section, paragraph, sentence, clause, or phrase shall be declared invalid or unconstitutional by the valid judgment or decree of a Court of competent jurisdiction, such invalidity or unconstitutionality shall not affect the validity or constitutionality of any of the remaining sections, paragraphs, sentences, clauses, or phrases of this Ordinance.

SECTION V: EFFECTIVE DATE. In the opinion of the City Council of North Logan, it is necessary that this ordinance take effect immediately; therefore, this Ordinance shall take effect immediately upon its passage and first posting as provided by law.

ADOPTED AND PASSED by the City Council of the City of North Logan, Utah, this 20th day of December, 2017.

NORTH LOGAN CITY

ATTEST:



Scott Bennett, City Recorder

By 

Lloyd Berentzen, Mayor

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CERTIFICATE OF DUE POSTING

I, SCOTT BENNETT, City Recorder of North Logan, Utah, hereby certify that I, on the 21st day of December, 2017, in the City of North Logan, Cache County, State of Utah, posted the foregoing Ordinance No. 17 - 09 in a likely manner, a copy of which is hereto attached, in each of three public places in the said City of North Logan, to-wit:

1. North Logan City Offices, 2076 North 1200 East
2. North Logan City Library, 475 East 2500 North
3. North Park Police Department, 575 East 2500 North

WITNESS my hand this 21st day of December, 2017.


Scott Bennett, City Recorder